



SHIRE OF CARNARVON  
**SCHEDULES**  
SPECIAL COUNCIL MEETING  
TUESDAY 10 JANUARY 2023

Council Chambers, Stuart Street  
CARNARVON, West Australia  
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# CONTENTS

## DEVELOPMENT & COMMUNITY

Schedule 4.1 (A) - Responsible Authority Report (RAR) .....	3
Schedule 4.1 (B) Attachment One The Pier Development Plans.....	29
Schedule 4.1 (C) Attachment Two Car Parking Assessment A.....	44
Schedule 4.1 (D) Attachment Three Car Parking Assessment B.....	47
Schedule 4.1 (E) Attachment Four Applicant Parking Demand Assessment .....	50
Schedule 4.1 (F) Attachment Five WAPC Cash-in-lieu Payment Calculation .....	64

## **Form 1: Responsible Authority Report**

(Regulation 12)

### **Notes for Author**

This template is provided to assist in the formulation of a Responsible Authority Report (RAR) for a Development Assessment Panel (DAP) application under Regulation 12 of the *Planning and Development (Development Assessment Panel) Regulations 2011*. This template should be used for all RARs under r.12 except for applications for a review of the original decision or a reconsideration of the original decision, please use the State Administrative Tribunal (SAT) Reconsideration template for those reports.

This template contains guidance to assist the author prepare a report that is comprehensive, succinct and addresses the key issues required for a decision to be made under the relevant Local Planning Scheme and Region Scheme (where applicable). The guidance notes should be deleted once the RAR is prepared and do not form part of the report.

The RAR template also provides for some sections to be deleted if they are not required. Where other sections are not relevant to a particular report and there are no specific notes, please insert either “not relevant” or “not applicable” under those headings.

Where the RAR template is submitted to a Council meeting, please note:

- The officer’s recommendation may be placed either first or last in accordance with the usual requirements of the Council meeting.
- For the submission of the RAR to the DAP Secretariat, the heading “Responsible Authority Recommendation” must contain the Council’s recommendation.
- If the officer recommendation is different from the Council recommendation, this should be shown under the heading “Officer Recommendation”.
- The “Reasons for Responsible Authority Recommendation” section should be completed after the Council meeting and should reflect why the recommendation differs from the reasons as shown in the minutes of the Council meeting.
- Where local government officers have delegation to provide the RAR to the DAP secretariat, only the “Responsible Authority Recommendation” should be completed and the “Officer Recommendation” section can be deleted.

# 85 OLIVIA TERRACE, CARNARVON - PROPOSED MIXED-USE DEVELOPMENT (HOTEL, RETAIL/COMMERCIAL, OFFICES, SHORT-STAY APARTMENTS AND MULTIPLE DWELLINGS)

## Form 1 – Responsible Authority Report (Regulation 12)

<b>DAP Name:</b>	Joint Development Assessment Panel	
<b>Local Government Area:</b>	Shire of Carnarvon	
<b>Applicant:</b>	Angovenp Pty Ltd ATF The Carnarvon Waterfront Unit Trust	
<b>Owner:</b>	Angovenp Pty Ltd	
<b>Value of Development:</b>	\$12 million <input checked="" type="checkbox"/> Mandatory (Regulation 5) <input type="checkbox"/> Opt In (Regulation 6)	
<b>Responsible Authority:</b>	Shire of Carnarvon	
<b>Authorising Officer:</b>	Stefan Louw,	
<b>LG Reference:</b>	P27/22; A4085, ADM1942	
<b>DAP File No:</b>	DAP/22/02363	
<b>Application Received Date:</b>	27 July 2022	
<b>Report Due Date:</b>	12 October 2022	
<b>Application Statutory Process Timeframe:</b>	90 Days	
<b>Attachment(s):</b>	1. Additional DA Information + Revised Development Plans 2. Car Parking Assessment A 3. Car Parking Assessment B 4. Applicant parking demand assessment 5. WAPC Method of Calculation (Cash-in-lieu Parking)	
<b>Is the Responsible Authority Recommendation the same as the Officer Recommendation?</b>  <b>[NB: To be advised following consideration and resolution of the application by the Carnarvon Shire Council]</b>	<input type="checkbox"/> Yes	Complete Responsible Authority Recommendation section
	<input type="checkbox"/> N/A	
	<input type="checkbox"/> No	Complete Responsible Authority and Officer Recommendation sections

### Responsible Authority Recommendation

That Council, by Simple Majority, pursuant to Regulation 12 of the Planning and Development (Development Assessment Panels) Regulations 2011 recommends that the Regional Joint Development Assessment Panel resolves to:

- Accept** that the DAP Application reference DAP/22/02363 is appropriate for consideration as a "Mixed Use development" land use and compatible with the objectives of the zoning table in accordance with Clause no.16 of the Shire of Carnarvon Local Planning Scheme No. 13.
- Approve** DAP Application reference DAP/22/02363 and accompanying revised set of development plans dated 9 December 2022 in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local*



*Planning Schemes) Regulations 2015*, and the provisions of the Shire of Carnarvon Local Planning Scheme No. 13, subject to the following conditions:

### Conditions

- (i) This decision constitutes planning approval only and is valid for a period of 2 years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect;
- (ii) The number of onsite car bays, bicycle bays and motorcycle/scooter bays determined to be provided within each stage of the proposed development, including access to them, shall be designed, constructed, sealed, drained, and marked to the specifications and satisfaction of the Shire of Carnarvon prior to occupancy of the respective stage of the development;
- (iii) The applicant shall prepare a legal agreement to facilitate reciprocal rights of vehicular access over adjoining lot 182 on P222239 to lot 300 on DP 412840 in which Shire of Carnarvon's interests are protected.

This agreement shall be prepared at no cost whatsoever to the Shire of Carnarvon.

Occupancy of the approved development shall not occur until the agreement is executed by the applicant and the Shire of Carnarvon;

- (iv) A cash-in-lieu payment equivalent to the provision of 19 car bays shall be made to the Shire of Carnarvon prior to occupancy of Stage 1 of the approved development.

The cash-in-lieu payment shall be calculated in accordance with the Western Australian Planning Commission's *Method of Calculation* publication;

- (v) The *porte cochere* structure shall be a cantilevered type with no supports located within the Robinson Street Road reserve. The *porte cochere* shall be designed constructed, sealed, drained, and marked to the specifications and satisfaction of the Shire of Carnarvon prior to occupancy of the approved hotel in Stage 2;
- (vi) The applicant shall submit to the Shire of Carnarvon a legal agreement for the registered proprietor (RP) of lot 300 on DP 412840 to:
  - (a) maintain the *porte cochere* within the Robinson Street reserve at its expense and to the satisfaction of the Shire of Carnarvon for the duration of the hotel use; and
  - (b) should the hotel use permanently cease, remove the *porte cochere* and make good at its expense to the satisfaction of the Shire of Carnarvon the land occupied by the *porte cochere* within 6 months of the hotel use permanently ceasing.

This agreement shall be prepared at no cost whatsoever to the Shire of Carnarvon.

Occupancy of the approved development shall not occur until the agreement is executed by the applicant and the Shire of Carnarvon;

- (vii) Prior to issue of a building permit for any stage of the development, a stormwater and drainage management plan shall be submitted to the specifications and satisfaction of the Shire of Carnarvon.

This plan shall specifically address the collection, storage and/or disposal of stormwater falling within the development site including roof run-off;

- (viii) Prior to occupancy of the development the approved stormwater management plan must be implemented to the satisfaction of the Shire of Carnarvon;
- (ix) Prior to occupancy of the approved development a waste management plan must be submitted to the specifications and satisfaction of the Shire of Carnarvon.

This plan is to specifically address the collection, storage and disposal of waste generated from within the approved development;

- (x) Prior to issue of a building permit for any stage of the development a landscape plan must be submitted to the satisfaction of the Shire of Carnarvon;
- (xi) Prior to occupancy of the approved development, the approved landscaping must be completed in accordance with the approved landscape plan;
- (xii) That there is to be no light spill emanating from corridors and common spaces within the approved development into the habitable rooms of adjoining lots;
- (xiii) Utilities servicing the approved development are to be designed to integrate with the overall architecture of the approved development;
- (xiv) Prior to occupancy of each stage of the approved development, each stage of the development shall be connected to a reticulated water supply to the requirements of the Water Corporation;
- (xv) Prior to occupancy of each stage of the approved development, each stage of the development shall be connected to the reticulated sewerage scheme, to the requirements of the Water Corporation;
- (xvi) Prior to occupancy of each stage of the approved development, each stage of the development shall be connected to the reticulated electricity network to the requirements of Horizon Power;
- (xvii) Filling of lot 300 on DP 412840 is prohibited, other than that directly necessary for the development as approved under a building permit;
- (xviii) A construction management plan to the specifications and satisfaction of the Shire of Carnarvon shall be submitted with a building permit application for the development.

This plan shall address:

- site accessibility;
- parking for contractors and 'tradies';
- ongoing dust control;
- provision and maintenance of construction site fencing and signage;
- location, unloading and storage of construction materials;
- storage and removal of construction waste;
- contingency arrangements for any construction works and securing materials during cyclone season; and
- contact details and availability of the project manager and site supervisor;

The approved construction management plan is to be implemented and adhered to at all times, up to the completion of the approved development and the issue of an occupancy permit by the Shire of Carnarvon;

- (xix) The proposed jetty and boat pens shall be designed to withstand/accommodate at least a 1 in 100-year cyclone event, and 1 in 500-year inundation levels;
- (xx) Occupancy within each stage of the approved development is not permitted until the respective occupancy permit has been issued by the Shire of Carnarvon pursuant to section 58 of the *Building Act 2011*;
- (xxi) No hotel suite (Stage 2) and no short-stay suites and studio suites in Stage 3 are to be occupied by any person for more than 3 months within any 12 month period.

To this end, the proprietor/manager of these suites is to maintain a register of guests, which is to be made available for inspection by authorised officers of the Shire of Carnarvon upon request.

#### **Advice Notes**

- (a) The design and construction of the proposed jetty and boat pens is to accord with applicable provisions of the *State Planning Policy (SPP) 2.6 – Coastal Planning*;
- (b) A licence is required to be obtained from the Department of Transport, Maritime Property Services to provide and maintain the proposed jetty and boat pens;
- (c) The proponent is advised that the Carnarvon Fascine waterway is managed and maintained by the Shire of Carnarvon. The Fascine waterway is a dynamic environment and maintaining navigable depths in and out of the waterway on an on-going basis cannot be guaranteed at this time. Accordingly, prospective registered proprietors of the multiple dwellings and/or the hotel, short-stay and studio suites seeking a boat pen

licence/lease should be formally notified of the vessel navigability situation by way of a Notification on the Certificate of Title or by some other appropriate means;

- (d) Access and facilities for people with disabilities must be provided at all times to all approved buildings, and internally throughout any building in accordance with AS1428.1 2009;
- (e) All building development is to comply with the National Construction Code Series;
- (f) A copy of the building plans and specifications must be deposited with the Department of Fire and Emergency Services (DFES) pursuant to regulation 18B of the *Building Regulations 2012*.

## Reasons for Responsible Authority Recommendation

Complete this section where the Council resolution differs from the Officer Recommendation and provide the reasons as shown in the Council minutes here. The Officer Recommendation section below, including reasons, will also need to be completed.

Include a brief summary of key issues and provide clear and succinct reason/s for the recommendation. If the recommendation is for a refusal, this section may be used to emphasise the reasons in the recommendation if required.

## Details: Outline of development application

Region Scheme	N/A	
Region Scheme - Zone/Reserve	N/A	
Local Planning Scheme	Shire of Carnarvon Local Planning Scheme 13	
Local Planning Scheme - Zone/Reserve	Regional Centre	
Structure Plan/Precinct Plan	N/A	
Structure Plan/Precinct Plan - Land Use Designation	N/A	
Use Class and permissibility:	<b>Proposed Use</b> <ul style="list-style-type: none"> <li>• Hotel/Motel</li> <li>• Retail</li> <li>• Office</li> <li>• Food and Beverage</li> <li>• Tavern</li> <li>• Small Bar</li> <li>• Short-stay Apartments</li> <li>• Permanent Residential Apartments</li> </ul>	<b>LPS 13 Use Class + Permissibility</b> <ul style="list-style-type: none"> <li>• <b>Hotel – ‘A’</b></li> <li>• <b>Shop – ‘P’</b></li> <li>• <b>Office – ‘P’</b></li> <li>• <b>Restaurant/Café – ‘P’</b></li> <li>• <b>Tavern – ‘D’</b></li> <li>• <b>Small Bar – ‘P’</b></li> <li>• <b>Serviced Apartment – ‘P’</b></li> <li>• <b>Multiple Dwelling – ‘P’</b></li> </ul>
Lot Size:	7,422 m <sup>2</sup>	
Existing Land Use:	Vacant + overflow vehicle parking	
State Heritage Register	No	
Local Heritage	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area	
Design Review	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other	
Bushfire Prone Area	No	
Swan River Trust Area	No	



### **Proposal:**

The proposal is for a staged mixed-use development (residential, retail, office, restaurant/bar, and short-stay/tourism accommodation) with associated amenities. Development is proposed to occur in 3 stages over a period of 2 – 4 years upon lot 300 on DP 412840, at the street address 85 Olivia Terrace Carnarvon.

#### Stage 1

Proposes development of a 4-storey multiple-dwelling complex comprising of 35 dwellings with the following mix of bedrooms/sizes:

- 7 dedicated x one-bedroom king-size (KS) dwellings – 52 m<sup>2</sup> floor area with 8.3m<sup>2</sup> (approx) balcony.
- 26 flexible x two-bedroom 'twin-key' dwellings – 79 m<sup>2</sup> floor area with 13.6 m<sup>2</sup> balcony.

NB: Each 'twin-key' dwelling is capable of being used as a separate self-contained dwelling, totalling **52 one-bedroom dwellings**.

Of these 52 dwellings, 26 comprise a king-size (KS) one-bedroom dwelling with a floor area of 52 m<sup>2</sup> and a balcony size of approximately 8.3 m<sup>2</sup>.

The remaining 26 comprise a studio (S) one-bedroom dwelling with a floor area of 27 m<sup>2</sup> and a balcony size of approximately 5.6 m<sup>2</sup>;

- 2 x two-bedroom dwellings – 72m<sup>2</sup> floor area with 68m<sup>2</sup> balcony/terrace.

#### Stage 2

Proposes development of a 4 storey hotel incorporating a lobby/reception, food and beverage premises (restaurant/bar), retail/commercial premises on the ground floor, commercial office space wholly on the second floor, and 49 hotel suites on the third and fourth floors.

Proposed floor sizes are as follows:

- Lobby/Reception – approx. 295 m<sup>2</sup> gross floor area (gfa)
- Food and beverage premises – approx. 480m<sup>2</sup> gfa
- Retail/commercial premises (west of Lobby/Reception) – approx. 213m<sup>2</sup> gfa
- Retail/commercial premises (east of Lobby/Reception) – approx. 425m<sup>2</sup> gfa
- Commercial office premises – approx. 1,700 m<sup>2</sup>

The 49 hotel suites comprise –

- 24 x one-bedroom king-size (KS) suites – 52 m<sup>2</sup> floor area with 8.3 m<sup>2</sup> (approx) balcony
- 22 x one-bedroom 'studio-type' (S) suites – 27m<sup>2</sup> floor area with 5.6 m<sup>2</sup> balcony
- 2 x two-bedroom suites – 72m<sup>2</sup> floor area with 68m<sup>2</sup> terrace
- 1 x four-bedroom suite – 144m<sup>2</sup> floor area with 136m<sup>2</sup> terrace

Layout of these suites will be configured in the same way as the proposed corresponding multiple dwellings developed in Stage 1.

A *porte cochere* (6.7m x 6m approx) – for the setting down and picking up of hotel occupants is proposed within the adjoining Robinson Street reserve, in front of the Hotel lobby/reception. This is proposed to be a cantilevered structure with no supporting columns/structures to be located within the Robinson Street reserve.

### Stage 3

Proposes development of a 3-storey short-stay apartment complex over a ground-level undercroft car park comprising of 63 suites with the following mix of bedrooms/sizes:

- 12 x one-bedroom king-size (KS) suites – 52 m<sup>2</sup> floor area with 8.3 m<sup>2</sup> (approx) balcony
- 12 x one-bedroom studio (S) suites – 27m<sup>2</sup> floor area with 5.6 m<sup>2</sup> balcony
- 39 x one-bedroom (single bed) studio suites (SS) – 20m<sup>2</sup> floor area (with no balcony)

Again, the 'king-size' (KS) suites and the 'studio-style' (S) suites will be configured in the same way as the proposed corresponding multiple dwellings layouts proposed in Stage 1.

A ground-level dedicated commercial delivery, loading, and set down area is proposed behind the rear of the ground floor retail and commercial premises (east of the Hotel lobby/reception), accessed via a proposed onsite car park.

Total nett lettable area (nla) for Stages 1,2 and 3 is approximately 6,192m<sup>2</sup>, which includes the hotel suites but excludes the multiple dwellings.

Other features of the proposed development include:

- A 1.8 metre wide footpath extending along and inside the development site's Robinson Street lot boundary – this footpath terminates at the food and beverage terrace, if walking westward along Robinson Street;
- A 1.8 metre wide footpath extending along the rear of the State 2 development complex, connecting to Olivia Terrace;
- Approximately 1,000m<sup>2</sup> (13.5 % of the development site) of communal open space incorporating a landscaped central courtyard and guest/resident swimming pool (approx. 765.5 m<sup>2</sup>) and a common outdoor kitchen/dining space (approx. 33 m<sup>2</sup>);
- A showering area for swimming pool patrons and residents;
- 35 private storage units (each approx. 4.25 m<sup>2</sup>);
- An outdoor bin and equipment store/compound (approx. 29 m<sup>2</sup>)
- A storage shed (approx. 104m<sup>2</sup>) incorporating 6 car bays;
- A 143 bay onsite car park (of which 40 bays will be undercover, including those proposed in the aforementioned storage shed);

- One (1) motor bike bay (with tilt guard);
- 7 bicycle bays (with side rails);
- Approximately 1,500m<sup>2</sup> (20% of site) of site landscaping; and
- An (offsite) floating jetty and 20 boat pens located in the adjacent Fascine waterway near the western end of Robinson Street, with access via a secured gatehouse.

A copy of the development plans together with additional information provided by the applicant's planning consultant (in response to queries raised in the development assessment) - are provided at **Attachment 1** to this report.

### **Background:**

The proposed development site is located at the southwestern end of Robinson Street (the main street within the Carnarvon townsite) where it intersects with Olivia Terrace via a roundabout. Olivia Terrace extends along the edge of the Fascine waterway, formerly the south arm of the Gascoyne River. A location/aerial photo of the development site is provided on page 6 of the development plans (see Attachment 1).

The development site is a prominent landmark site within the Carnarvon town centre, with outlooks onto Robinson Street, along Olivia terrace, the Fascine waterway, and across to Babbage and Whitlock Islands.

Its survey description is lot 300 on Deposited Plan 412840, in the land district of Gascoyne. It is generally rectangular in shape and flat.

The development site previously contained the Carnarvon Police Station and Court House. The development site is presently vacant with some remnant palm trees located along its Olivia Terrace frontage and within the south-west corner of the site, and some remnant river gums along its Robinson Street frontage towards its eastern corner.

Along its northwest boundary the development site adjoins an existing one-storey single house (77 Olivia Terrace) and the one-storey State Emergency Service (SES) premises (9 Camel Lane). Along its northeast boundary it adjoins the Carnarvon Civic Centre car park (1 Camel Lane) and the two-storey Bankwest premises (15 Robinson Street).

The development site is exposed to prevailing south-south-westerly winds, travelling up the Fascine waterway from the coast, rendering the site quite windy. The development responds to the impact of these prevailing winds by an enclosed space containing a restaurant/café on the ground floor and upper floor terraces.

Robinson Street is a sealed, wide boulevard style main street with a landscaped median incorporating central, angled vehicle parking bays. Parallel vehicle parking bays are also provided along and within its street verges.

The street is framed by existing retail, commercial and public buildings, no more than 2 storeys high. These buildings comprise a mix of building styles and type, ranging from the Federation-era through to modern contemporary.

Olivia Terrace is a two-lane sealed road, framed by existing single houses and some tourist (short-stay) apartments/units along its eastern edge. A landscaped pedestrian boulevard, grassed public open space, and the adjoining Fascine waterway form the western edge of Olivia Terrace.

An existing public car park is located directly opposite the development site. This is a popular car park used by travellers and attendees of passive recreation activities, and community events conducted along the Fascine waterway edge.

### **Legislation and Policy:**

#### Legislation

- *Planning and Development (Local Planning Schemes) Regulations 2015*  
(cf clauses 62, 63, 63A, 64, 64A, 65A, 65B, 66, 67 and 71)
- *Shire of Carnarvon Local Planning Scheme (LPS) 13*  
(cf clauses 9, 17, 18, 25, 29, 32, 34, 37, 38 and Schedule 5—clauses 1, 4.1, 4.3, 7, 8.1, 8.2, 10.1, 14, 17, 18, 19, 20, 21, 22(d), 23, 24, 31 and 35)

#### State Government Policies

- *State Planning Policy (SPP) 1: State Planning Framework*  
(cf provisions 1, 3.1(b), 3.1(f) - (i) and (iii), and 5)
- *State Planning Policy (SPP) 3: Urban Growth and Settlement*  
(cf provisions 3, 4, and 5.1)
- *State Planning Policy (SPP) 3.4: Natural Hazards and Disasters*  
(cf provisions 3, 4, and 5.1—Severe Storms and Cyclones, and Storm Surge)
- *State Planning Policy (SPP) 7.0: Design of the Built Environment*  
(cf provisions 4, 5, and 6—Design Principles)
- *State Planning Policy (SPP) 7.3: Residential Design Codes—Apartments*  
(cf Parts 1 – 4)

#### Structure Plans/Activity Centre Plans

None applicable

#### Local Policies

The Shire has no adopted Local Planning Policies

### **Consultation:**

#### Public Consultation

The development proposal was publicly advertised pursuant to clauses 64 (1) (b) (i) and (iii), and 64 (3) (a) and (b) contained in Schedule 2 of the *LPS Regs 2015*, for a period of 28 days.

At the conclusion of the public advertising period a total of **two (2) submissions** had been received.

Both submissions raised only **one key issue** - being the amount of parking proposed by the development (100 bays as initially proposed), which given the nature and type of proposed development, the submitters considered unsatisfactory.

(NB: The initial development plans submitted with the development application denoted 100 onsite car bays. Following initial assessment of the development application and the public consultation phase, revised plans were submitted denoting 143 onsite car bays. It is the revised plans at Attachment 1 for which development approval is now sought).

The submitters contend that the lack of sufficient onsite car parking bays for the proposed development will exacerbate existing parking congestion/demand already being experienced within Robinson Street, especially given that the Shire – some time ago - removed 6 previous on-street car bays from within the Robinson Street reserve at the front of their premises on Robinson Street.

The submitters did not state when this parking congestion/demand was specifically occurring but presumably it is during daylight hours and during peak traffic periods.

Concern about the lack of sufficient onsite vehicle parking is further discussed in the **Planning Assessment** section of this report.

#### Referrals/consultation with Government/Service Agencies

The application was referred to the following Government/Service agencies for consideration and comment:

##### **DEPARTMENT OF TRANSPORT**

A copy of the original and subsequent revised development plans were referred to the Department of Transport (DoT) for its consideration and comment.

The DoT limited its consideration and comments to the proposed jetty and boat-pens associated with the development. It did not consider the landside development land uses nor any aspect of the built form beyond the impact of forecast storm surge and sea level rise.

The DoT advised as follows in response to the revised development plans:

- (a) Construction of both the water and landside components of the development shall respond to and comply with the recommendations of the WA Planning Commission's *Statement of Planning Policy 2.6 – Coastal Planning*;
- (b) The proponent shall apply for and obtain a jetty licence from the Department of Transport, Maritime Property Services;
- (c) The proposed pier and pens shall be designed to withstand/accommodate at least 1 in 100-year cyclonic conditions and 1 in 500-year inundation levels;
- (d) The proponent is advised that the Carnarvon Fascine, managed and maintained by the Shire of Carnarvon, is a dynamic environment and maintaining navigable depths in and out of the Fascine on an on-going basis cannot be guaranteed at this time; accordingly prospective guests and resident purchasers should be formally notified of the vessel navigability situation by way of a Notification on Title and/or using some other appropriate mechanism.



The DoT considers items (a), (b) and (d) above are best provided as 'Advice Notes' on any development approval granted, and if development approval is granted, item (c) be imposed as a condition of development approval.

It is understood that the proponent is accepting of the aforementioned DoT advice/recommendations.

#### **WATER CORPORATION**

A copy of the original development plans was referred to the Water Corporation for its consideration and comment.

The Water Corporation confirmed that there is capacity in the network to serve the proposed development. It further recommended that the applicant engage a suitably qualified engineering consultant to advise on the options available.

Appropriate conditions have been included to address this matter.

#### **Design Review Panel Advice**

No formal design review was undertaken either prior to or after lodgement of the development application although a presentation was made to the Carnarvon Shire Council on the development proposal by the applicant prior to lodgement of the development application.

Following lodgement of the development application, a preliminary assessment of the development proposal was undertaken to firstly, ascertain that the requisite information (including design information) required to accompany the development application had been provided in accordance with clause 63 of Schedule 2 in the *LPS Regs 2015*; and secondly, to identify any key deficiencies in respect to conformance with key planning requirements (as prescribed in LPS 13).

An initial and subsequent list of matters was forwarded to the applicant following the preliminary and a subsequent complete assessment of the development proposal.

The applicant responded accordingly with updated/revised development plans.

#### **Other Advice**

##### **SHIRE EXECUTIVE MANAGER INFRASTRUCTURE:**

Recommended development approval conditions satisfy Infrastructure Services requirements.

##### **CITY OF GREATER GERALDTON'S COORDINATOR OF BUILDING SURVEYING:**

(Provides building surveyor services to the Shire)

Recommends the following standard advice notes be attached to the granting of development approval—

- (i) Access and facilities for people with disabilities must be provided at all times to the building and internally throughout the building in accordance with AS1428.1 2009  
(Shire advice note BA1);
- (ii) The development is to comply with the National Construction Code Series

(Shire advice note BA3);

- (iii) Plans and specifications for the proposed development must be deposited with the Department of Fire and Emergency Services (DFES) pursuant to regulation 18B of the Building Regulations 2012.

(Shire advice note BA5)

### **Planning Assessment:**

Assessment of the proposed development was carried out against applicable legislative and policy provisions listed in the **Legislation and Policy** section of this report.

The assessment identified the following key considerations in the determination of this application:

#### **MULTIPLE DWELLINGS (STAGE 1)**

##### *Ground Floor Occupancy*

Multiple dwellings are proposed at ground level, which is contrary to **clause 35.4 in LPS 13**. However, as these front Olivia Terrace which also has existing and adjacent housing and short-stay apartments located further north along and on the same side of Olivia Terrace, this arrangement is deemed acceptable.

It is understood the principal purpose of clause 35.4 in LPS 13 was to preclude residential dwellings from occupying **prime commercial ground floor space** (principally along Robinson Street) within the Carnarvon town centre.

##### *Built Form, Landscape Character, and Streetscapes*

It should be noted that the Shire has no prescribed built form, landscape character, or streetscape guidelines against which the design, form, scale, external appearance (facades) etc of the proposed multiple dwelling development can be properly and objectively assessed (*cf* **Element 2.5 – Plot Ratio in the R-Codes Vol.2**).

##### *Building Height*

Although the R-Codes Vol 2 prescribe a maximum height limit of 3 storeys for multiple dwelling development where an R60 coding applies (*cf* **Element 2.2 – Building Height of the R-Codes Vol. 2**), both the codes and LPS 13 allow a maximum height limit of 12 metres.

Except for the proposed rooftop building services and solar arrays, the proposed buildings for each stage of the development will be no more than 12 metres high.

##### *Building Separation*

Whilst the proposed multiple dwelling development is separated only 1.05 metres away from the Stage 2 development block, it is deemed that the prescribed objective of providing for residential amenity including visual and acoustic privacy, natural ventilation, sunlight and daylight access and outlook will be met (*cf* **Element 2.7 - Building Separation of the R-Codes Vol.2**).

##### *Natural Ventilation*

(cf Element 4.2 in the R-Codes Vol 2)

Although just over 60% of the proposed dwellings have been assessed as being naturally ventilated (principally the dwellings fronting Olivia Terrace and the northern end dwellings facing into the central courtyard), arguably, natural ventilation of the remaining dwellings facing the central courtyard could be problematic because the openings in these are directly opposite the openings of the dwellings fronting Olivia Terrace and will rely on the latter openings being opened to facilitate natural ventilation. This circumstance, however, will be outside the control of the occupants in the dwellings facing the central courtyard.

To this end, ventilation stacks are proposed within each dwelling to provide an escape for warm air. This is deemed acceptable.

#### *Size and Layout of Dwellings*

(cf Element 4.3 in the R-Codes Vol 2)

Table 4.3a in **Acceptable Outcome A4.3.1** prescribes a one-bedroom 'studio' (S) dwelling to have a **minimum floor area of 37m<sup>2</sup>**.

The proposed one-bedroom studio (S) dwelling (27 m<sup>2</sup>) – when **occupied as a stand-alone, self-contained separate dwelling to the one-bedroom king-size (KS) dwelling** does not accord with this minimum floor area.

However, the applicant submits that despite not complying with the **prescribed minimum floor size of 37 m<sup>2</sup>**, the design and layout of these studio dwellings accords with **Design Guidance DG 4.3.1** of the R-Codes Vol 2 in that the design:

- (i) as a 'dual-key apartment' provides more flexibility for different household configurations; and
- (ii) illustrates potential furniture layouts using realistically sized furniture, including living and dining settings in the living space, enabling an occupant to adequately move around and within the dwellings.

#### *Private Open Space and Balconies*

(cf Element 4.4 in the R-Codes Vol 2)

**Acceptable Outcome A4.4.1** in the R-Codes Vol 2 prescribes a one-bedroom 'studio' (S) dwelling to have a **minimum balcony floor area of 8 m<sup>2</sup>**. The proposed studio (S) dwelling balconies are denoted as having a balcony floor area of 5.6 m<sup>2</sup>.

The applicant submits that this lesser balcony floor size is **compensated by the provision of 1,000 m<sup>2</sup> of communal open space** as well as the ground-floor dwellings having generously sized private outdoor spaces.

#### *Circulation and Common Spaces*

(cf Element 4.5 in the R-Codes Vol 2)

**Acceptable Outcome A4.5.1** of the R-Codes Vol 2 prescribes circulation corridors to be a **minimum width of 1.5 metres**. The proposed corridor widths adjacent to the stairs and lifts have been measured at 1.4 metres wide when scaled from the accompanying floor plans.

This slightly lesser width will not adversely impact universal access and thus ought to be deemed acceptable.

Additionally, no details have been provided to assess conformance with **Acceptable Outcome A4.5.4** which prescribes that there is to be no light spill emanating from corridors and common spaces into the habitable rooms of adjacent dwellings.

It is recommended that a condition of development approval be imposed requiring that there is to be no light spill emanating from corridors and common spaces into the habitable rooms of adjoining lots.

#### *Storage*

(cf Element 4.6 in the R-Codes Vol 2)

**Acceptable Outcome A4.6.1** of the R-Codes Vol 2 prescribes a separate 3m<sup>2</sup> minimum sized, ventilated, weatherproof, bulky goods storage area for each multiple dwelling.

A total of 35 storage areas are proposed - one for each multiple dwelling should the proposed 26 flexible x two-bedroom 'twin-key' dwellings be accepted in lieu of the proposed one-bedroom king-size (KS) dwellings, the one-bedroom studio (S) dwellings, and the 2 x two-bedroom dwellings being counted as 61 multiple dwellings.

Based on the above, it follows that the proposal for 35 storage units be accepted too.

#### *Managing Noise Impacts*

(cf Element 4.7 in the R-Codes Vol 2)

No details have been provided in respect to proposed acoustic treatments to assess for conformance with this element's objectives and acceptable outcomes. The current noise regulations ought to be able to address any noise impacts that might emanate from the proposed development.

#### *Dwelling Mix*

(cf Element 4.8 in the R-Codes Vol 2)

**Acceptable Outcome A4.8.1 (b)** in the R-Codes Vol 2 prescribes that **at least 20% of apartments have differing bedroom numbers.**

The proposed multiple dwelling development denotes provision and flexibility for one and two-bedroom dwellings. The development does not propose any dwellings specifically designed with 3 or more bedrooms.

Whilst the Shire has no adopted housing strategy for the town of Carnarvon, the Shire's *Local Planning Strategy* identified a lack of 3+ bedroom dwellings within Carnarvon. Despite the apparent need for this housing type such a requirement is not mandatory nor can a proponent be compelled to provide this.

#### *Universal Design*

(cf Element 4.9 in the R-Codes Vol 2)

The proposed multiple dwelling development has not been assessed against the objective of this element.

It is suggested that it is more appropriate to assess this element at the building permit application stage and that no building permit be issued for the multiple dwelling development until this assessment has been completed and any matters arising have been addressed to the Shire's satisfaction.

#### *Landscape Design*

(*cf* **Element 4.12 in the R-Codes Vol 2**)

The proposed landscaping scheme in respect to the multiple dwelling development has not been assessed against the plant selection objective (**Element 04.12.2**) nor the water harvesting/re-use objective (**Element 04.12.3**) of this element.

It is considered that these elements be assessed as part of the assessment of a site landscaping plan which will be required (via a condition of development approval) to be prepared and submitted for Shire approval and implementation thereafter.

#### *Energy Efficiency*

(*cf* **Element 4.15 in the R-Codes Vol 2**)

The proposed multiple dwelling development has not been assessed against relevant objectives of this element.

It is suggested that it is more appropriate to assess this element at the building permit application stage and that no building permit be issued for the multiple dwelling development until this assessment has been completed and any issues arising have been addressed to the Shire's satisfaction.

#### *Water Management and Conservation*

(*cf* **Element 4.16 in the R-Codes Vol 2**)

No details have been provided in respect to the collection, storage and/or disposal of stormwater falling within the development site, including in respect to the proposed multiple dwelling development.

It is recommended that a condition of development approval be imposed requiring the submission and implementation of an onsite stormwater management plan for the whole of the proposed development.

#### *Waste Management*

(*cf* **Element 4.17 in the R-Codes Vol 2**)

The proposed waste bin compound will be visibly shielded from Olivia Terrace for the storage of waste bins, which is considered acceptable. However, no detail was provided on the management of waste disposal for the development. This will be addressed via a condition of development approval.

#### *Utilities*

(*cf* **Element 4.18 in the R-Codes Vol 2**)

No details have been provided in respect to the location and design of utility services infrastructure that will service the multiple dwelling development to enable an



assessment of how well the infrastructure will integrate with the proposed multiple dwelling development.

It is recommended that these details be requested via a condition of development approval for further assessment and approval.

### **PROPOSED STUDIO SUITES (STAGE 3)**

The best use class as defined in LPS 13 that could be applied to these rooms is either a **‘motel’** or alternatively as **‘tourist development’** as they are part of a proposed complex offering onsite facilities for the use of guests as well as facilities for the management of the development.

Under either of these classifications the rooms can only be **occupied on a short-stay basis**.

### **ONSITE STORMWATER DRAINAGE**

No details have been provided with the application for the collection, storage and/or disposal of stormwater falling within the development site.

It is recommended that a condition of development approval be imposed requiring the submission and implementation of an onsite stormwater management plan.

### **SEWERAGE DISPOSAL**

Likewise, no details have been provided with the application for the disposal of onsite generated sewerage (wastewater). It is understood that connection to the town’s sewerage network is available, and hence will be required.

It is recommended that a condition of development approval be imposed requiring each stage of the development to be connected to the town’s sewerage network prior to occupancy of any building within each stage.

### **POTABLE WATER SUPPLY**

It is understood that connection to the town’s water supply network is available, and hence will be required. There is an existing water supply pipeline and associated infrastructure presently located within the development site which will require relocation at the developer’s expense (and prior to construction of Stage 1 of the development).

### **EXTERNAL SITE LIGHTING**

No details have been provided with the application detailing any proposed external lighting.

To this end, it is recommended that a condition of development approval be imposed requiring that there be no light spill emanating from corridors and common spaces within the development into habitable rooms of adjoining lots.

### **PORTE COCHERE**

**Clause 22 in Schedule 5—Additional Site and Development Requirements** to LPS 13 prescribes that set-down areas - such as the proposed *porte cochere* - shall not be located within a public road reserve.

The proposed location/siting of the *porte cochere* is contrary to this.

Notwithstanding, **clause 34(2)** of LPS 13 allows approval of a development application that does not comply with any additional site and development requirement. Further, as the proposed *porte cochere* is a cantilevered type structure, no supporting columns will be required within the Robinson Street road reserve.

The location of the proposed *porte cochere* is likely to result in the loss of at least 3 parallel parking bays within the adjacent Robinson Street reserve, which will either need to be provided elsewhere within the Carnarvon town centre by the proponent or alternatively **make a cash-in-lieu payment** to the Shire in addition to the 19 car bays shortfall.

### **VEHICLE ACCESS**

Vehicle access to and from the development site is proposed off Olivia Terrace at the north west corner of the development site and off the rear driveway within lot 182 on P222239 which services the Carnarvon Civic Centre (being Crown Reserve 21853 which is vested in the Shire).

It should be noted that the rear driveway to the Carnarvon Civic Centre is not a public road nor a public right-of-way.

It is recommended that use of this driveway by the proponent be **subject to an access agreement with the Shire**, which ought to include a proportional monetary contribution by the proponent towards the upkeep and maintenance of the driveway whilst the driveway provides access to the development site.

### **CAR PARKING**

The development proposes 143 onsite bays, configured in a two-storey car park located along the northern boundary of the subject land and in a ground-level undercroft proposed under the proposed short-stay suites and studio-suites (SS) in Stage 3.

Based on the prescribed onsite car parking provisions in LPS 13 a minimum of **222** onsite car bays have been calculated as being required for the proposed **restaurant/café** (49 bays), **shops** (33 bays), **offices** (28 bays), **hotel** (49 bays), **short-stay apartments** (24 bays) and the **short-stay studio suites** (33 bays).

For the proposed 61 **multiple dwellings** (if all of the proposed 1KS, 1B and 2B suites are counted as separate self-contained dwellings), a minimum of **73 bays** are required as per the applicable R-Codes provisions.

Thus, a total of **295 onsite car bays** have been calculated as being required, resulting in a **shortfall of 152 onsite bays**.

**Attachment 2** to this report sets out the calculated base car parking, bicycle, and motorcycle/scooter parking required (**Car Parking Assessment A**).

In the interest of avoiding the aesthetics of a large 'parking lot' being located within the Carnarvon town centre, and having regard to an allowance for reciprocal parking, average occupancy rates for short-stay accommodation, and that other modes of transport may be used to access and utilise the various uses within the proposed

development (walking, bicycles, coaches etc), some concessions as to the amount of onsite car parking required is worthy of consideration.

For example, car parking required for the **hotel and short-stay uses** could be based upon a **60% occupancy rate**. For all the **other commercial uses** a **50% occupancy rate** could be applied.

With these concessions a minimum total of **197 bays** would be required, resulting in a **revised shortfall of 54 bays**.

(NB: It should be noted that following initial assessment of the development application and consideration of the aforementioned concessions, the applicant was erroneously advised that the minimum number of onsite car bays required for the multiple dwelling development was **61 bays** (the number of proposed dwellings) instead of **73 bays**, resulting in a **minimum total of 185 bays** required for the whole of the proposed development).

The proponent argues that the minimum onsite car parking requirement for the multiple dwelling component ought to be based upon the provision of **35 multiple dwellings only**. On this basis, a minimum of **49** onsite bays have been calculated as being required for the multiple dwelling development.

Adding the 222 minimum bays calculated for the other proposed uses to the above calculated requirements results in a total of **271** onsite bays being required. This results in a **shortfall of 128 onsite bays**. **Attachment 3** to this report sets out the applicant's 'base' onsite parking calculation (**Car Parking Assessment B**).

Notwithstanding the calculated shortfall in the provision of onsite car parking, the applicant has requested consideration of allowing for reciprocal parking on the basis that tenants or occupants of the multiple dwellings and/or the hotel and short-stay apartments are likely to consume food and beverages at the proposed restaurant/bar, and possibly access the commercial retail and office spaces, as will other patrons coming from elsewhere in Carnarvon who may park their vehicles in Robinson Street or surrounding streets.

It should be noted that parking within Robinson Street is usually at a premium during the day but is rarely fully occupied in the evening at which time the restaurant/bar – and perhaps other commercial premises within the development - will be open to the public.

In consideration of this the applicant was requested to undertake a **parking demand assessment** to confirm likely parking demand. A copy of this assessment is provided at **Attachment 4** to this report.

Based on its parking demand assessment of 97 bays at peak periods (weekdays between 12 and 1.00 pm) and Saturday (less than the weekday peak), the applicant argues and advocates that the provision of 143 onsite bays as proposed is more than sufficient to meet envisaged car parking demand.

It should be further noted that an independent verification of this assessment has not been carried out by the Shire, however, the Shire's Manager Regulatory Services recommends that **162 bays** ought to be the minimum number of calculated/assessed

bays required—accepting the applicant’s argument for **49 bays** to be provided for 35 proposed multiple dwellings in addition to allowing for a reduction of **23 bays** as calculated by the applicant based on erroneous advice provided by the Shire in initially calculating onsite parking bays required).

This results in a shortfall of **19 bays** which can be provided as a **cash-in-payment**.

It is recommended that in calculating any cash-in-lieu payment required the WAPC’s *Method of Calculation* publication be applied (cf **Attachment 5** to this report).

### **BICYCLE AND MOTORCYCLE/SCOOTER PARKING**

Based upon 35 proposed multiple dwellings and that each proposed multiple dwelling store can accommodate the storage of at least one bicycle/scooter, the **minimum requirements for the provision of onsite bicycle and scooter parking are satisfied**.

Although only **one motorcycle bay is proposed** the applicant submits that this provision is **consistent** with the **objectives of Element 3.9** in the R-Codes Vol 2. This is deemed acceptable.

### **Conclusion:**

In conclusion, it can be stated that the proposal has merit, especially since the nature of the proposed development and its range and mix of proposed uses accord with several objectives of the Regional Centre zoning that applies to the land, as well as with a number of the aims of LPS 13.

From an urban design perspective, the sleeving of the proposed buildings along Robinson Street (Stage 2) and Olivia Terrace (Stage 1), resulting in the proposed onsite car parking area being located behind and screened from these streets is welcomed and supported.

This arrangement particularly accords with the Regional Centre zone objective to ‘...encourage pedestrian-friendly, street-oriented development that responds to and enhances the key elements of the Regional Centre and to develop areas for public interaction’.

In considering the merit of the proposed development it should be noted that:

In respect to **LPS 13**—

- (i) The proposed location of the multiple dwellings on the ground floor is contrary to the scheme provision for multiple dwellings to be limited to upper floors of a building (but this requirement can be varied/dispensed with);
- (ii) The proposed location of the *porte cochere* is contrary to the scheme provision stipulating that all set down areas shall not be provided within a public road reserve (this requirement too can be varied/dispensed with);
- (iii) The amount of proposed onsite car bays is **19 bays short** of the **162** car bays calculated as being required (similarly, this requirement can be varied/dispensed with);

- (iv) There is a lack of detail or advice in respect to the drainage and disposal of stormwater; onsite waste management; internal and external lighting and whether there will be any light overspill; water management and conservation; and how utility services will be integrated into the design of the buildings.

Such advice, however, can be provided and detailed via the imposition of appropriate conditions should development approval be granted.

In respect to the **R-Codes—**

The multiple dwelling development does not accord with the following **prescribed acceptable outcomes** of the *R-Codes Vol 2*:

- (i) The multiple dwellings fronting the proposed central courtyard are not optimised for the provision of natural ventilation (but are provided with stack ventilation);
- (ii) The studio (S) dwellings will not meet the minimum floor area (37m<sup>2</sup>) and balcony size (8m<sup>2</sup>) if occupied separately and used in a self-contained, stand-alone way to the one-bedroom king-size (KS) dwellings but from the typical floor plans provided (including furniture layout) it appears that movement around and within the studio dwelling by an occupant will be satisfactory;
- (iii) The corridors adjacent to the lifts and stairs do not meet the minimum corridor width of 1.5 metres (but they are denoted as being 1.4 metres in width which is considered acceptable);
- (iv) If all of the proposed multiple dwellings are counted as 61 dwellings (and not 35) there is a shortfall of 26 storage units;

Notwithstanding that the above aspects may not accord with relevant acceptable outcomes they largely accord with the corresponding Element Objectives and relevant Design Guidance listings.

### **Officer Recommendation**

It is recommended that the Joint Regional Development Assessment Panel resolves to:

1. **Accept** that the DAP Application reference DAP/22/02363 is appropriate for consideration as a “Mixed Use development” land use and compatible with the objectives of the zoning table in accordance with Clause no.16 of the Shire of Carnarvon Local Planning Scheme No. 13.
2. **Approve** DAP Application reference DAP/22/02363 and accompanying revised set of development plans dated 9 December 2022 in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the Shire of Carnarvon Local Planning Scheme No. 13, subject to the following conditions:

### **Conditions**

- (i) This decision constitutes planning approval only and is valid for a period of 2 years from the date of approval. If the subject development is not substantially



commenced within the specified period, the approval shall lapse and be of no further effect;

- (ii) The number of onsite car bays, bicycle bays and motorcycle/scooter bays determined to be provided within each stage of the proposed development, including access to them, shall be designed, constructed, sealed, drained, and marked to the specifications and satisfaction of the Shire of Carnarvon prior to occupancy of the respective stage of the development;
- (iii) The applicant shall prepare a legal agreement to facilitate reciprocal rights of vehicular access over adjoining lot 182 on P222239 to lot 300 on DP 412840 in which Shire of Carnarvon's interests are protected.

This agreement shall be prepared at no cost whatsoever to the Shire of Carnarvon.

Occupancy of the approved development shall not occur until the agreement is executed by the applicant and the Shire of Carnarvon;

- (iv) A cash-in-lieu payment equivalent to the provision of 19 car bays shall be made to the Shire of Carnarvon prior to occupancy of Stage 1 of the approved development.

The cash-in-lieu payment shall be calculated in accordance with the Western Australian Planning Commission's *Method of Calculation* publication;

- (v) The *porte cochere* structure shall be a cantilevered type with no supports located within the Robinson Street Road reserve. The *porte cochere* shall be designed constructed, sealed, drained, and marked to the specifications and satisfaction of the Shire of Carnarvon prior to occupancy of the approved hotel in Stage 2;
- (vi) The applicant shall submit to the Shire of Carnarvon a legal agreement for the registered proprietor (RP) of lot 300 on DP 412840 to:
  - (a) maintain the *porte cochere* within the Robinson Street reserve at its expense and to the satisfaction of the Shire of Carnarvon for the duration of the hotel use; and
  - (b) should the hotel use permanently cease, remove the *porte cochere* and make good at its expense to the satisfaction of the Shire of Carnarvon the land occupied by the *porte cochere* within 6 months of the hotel use permanently ceasing.

This agreement shall be prepared at no cost whatsoever to the Shire of Carnarvon.

Occupancy of the approved development shall not occur until the agreement is executed by the applicant and the Shire of Carnarvon;

- (vii) Prior to issue of a building permit for any stage of the development, a stormwater and drainage management plan shall be submitted to the specifications and satisfaction of the Shire of Carnarvon.

This plan shall specifically address the collection, storage and/or disposal of stormwater falling within the development site including roof run-off;

- (viii) Prior to occupancy of the development the approved stormwater management plan must be implemented to the satisfaction of the Shire of Carnarvon;
- (ix) Prior to occupancy of the approved development a waste management plan must be submitted to the specifications and satisfaction of the Shire of Carnarvon.

This plan is to specifically address the collection, storage and disposal of waste generated from within the approved development;

- (x) Prior to issue of a building permit for any stage of the development a landscape plan must be submitted to the satisfaction of the Shire of Carnarvon;
- (xi) Prior to occupancy of the approved development, the approved landscaping must be completed in accordance with the approved landscape plan;
- (xii) That there is to be no light spill emanating from corridors and common spaces within the approved development into the habitable rooms of adjoining lots;
- (xiii) Utilities servicing the approved development are to be designed to integrate with the overall architecture of the approved development;
- (xiv) Prior to occupancy of each stage of the approved development, each stage of the development shall be connected to a reticulated water supply to the requirements of the Water Corporation;
- (xv) Prior to occupancy of each stage of the approved development, each stage of the development shall be connected to the reticulated sewerage scheme, to the requirements of the Water Corporation;
- (xvi) Prior to occupancy of each stage of the approved development, each stage of the development shall be connected to the reticulated electricity network to the requirements of Horizon Power;
- (xvii) Filling of lot 300 on DP 412840 is prohibited, other than that directly necessary for the development as approved under a building permit;
- (xviii) A construction management plan to the specifications and satisfaction of the Shire of Carnarvon shall be submitted with a building permit application for the development.

This plan shall address:

- site accessibility;
- parking for contractors and 'tradies';
- ongoing dust control;

- provision and maintenance of construction site fencing and signage;
- location, unloading and storage of construction materials;
- storage and removal of construction waste;
- contingency arrangements for any construction works and securing materials during cyclone season; and
- contact details and availability of the project manager and site supervisor;

The approved construction management plan is to be implemented and adhered to at all times, up to the completion of the approved development and the issue of an occupancy permit by the Shire of Carnarvon;

- (xix) The proposed jetty and boat pens shall be designed to withstand/accommodate at least a 1 in 100-year cyclone event, and 1 in 500-year inundation levels;
- (xx) Occupancy within each stage of the approved development is not permitted until the respective occupancy permit has been issued by the Shire of Carnarvon pursuant to section 58 of the *Building Act 2011*;
- (xxi) No hotel suite (Stage 2) and no short-stay suites and studio suites in Stage 3 are to be occupied by any person for more than 3 months within any 12 month period.

To this end, the proprietor/manager of these suites is to maintain a register of guests, which is to be made available for inspection by authorised officers of the Shire of Carnarvon upon request.

#### **Advice Notes**

- (a) The design and construction of the proposed jetty and boat pens is to accord with applicable provisions of the *State Planning Policy (SPP) 2.6 – Coastal Planning*;
- (b) A licence is required to be obtained from the Department of Transport, Maritime Property Services to provide and maintain the proposed jetty and boat pens;
- (c) The proponent is advised that the Carnarvon Fascine waterway is managed and maintained by the Shire of Carnarvon. The Fascine waterway is a dynamic environment and maintaining navigable depths in and out of the waterway on an on-going basis cannot be guaranteed at this time. Accordingly, prospective registered proprietors of the multiple dwellings and/or the hotel, short-stay and studio suites seeking a boat pen licence/lease should be formally notified of the vessel navigability situation by way of a Notification on the Certificate of Title or by some other appropriate means;
- (d) Access and facilities for people with disabilities must be provided at all times to all approved buildings, and internally throughout any building in accordance with AS1428.1 2009;

- (e) All building development is to comply with the National Construction Code Series;
- (f) A copy of the building plans and specifications must be deposited with the Department of Fire and Emergency Services (DFES) pursuant to regulation 18B of the *Building Regulations 2012*.

#### Reasons for Officer Recommendation

Provide a brief summary of key issues and provide clear and succinct reason(s) for the Officer Recommendation. It should reflect why the recommendation differs from the reasons as shown in the minutes of the Council meeting. If the recommendation is for a refusal, this section could be used to amplify the reasons in the recommendation if required.

# THE PIER

## CARNARVON

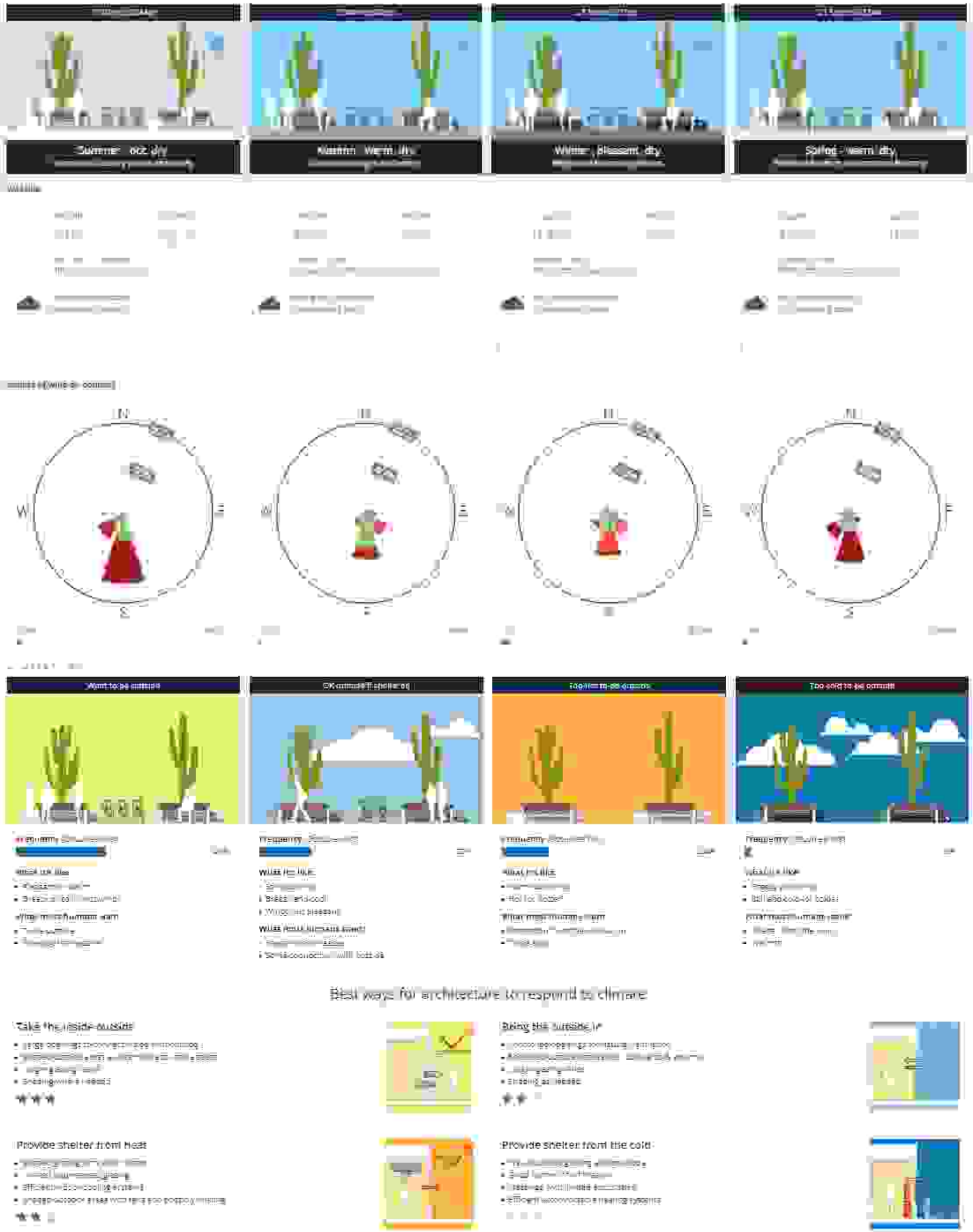




GENERAL NOTES:

1	USE FIGURED DIMENSIONS WHERE SHOWN.
2	COLOURS AND TEXTURES USED IN THIS DOCUMENT ARE FOR THE PURPOSE OF CONTRAST AND COMMUNICATION AND ARE NOT; EXCEPT WHERE SPECIFICALLY ANOTATED; INDICATIVE OF FINAL FINISHES OR COLOURS.

PRE DESIGN:

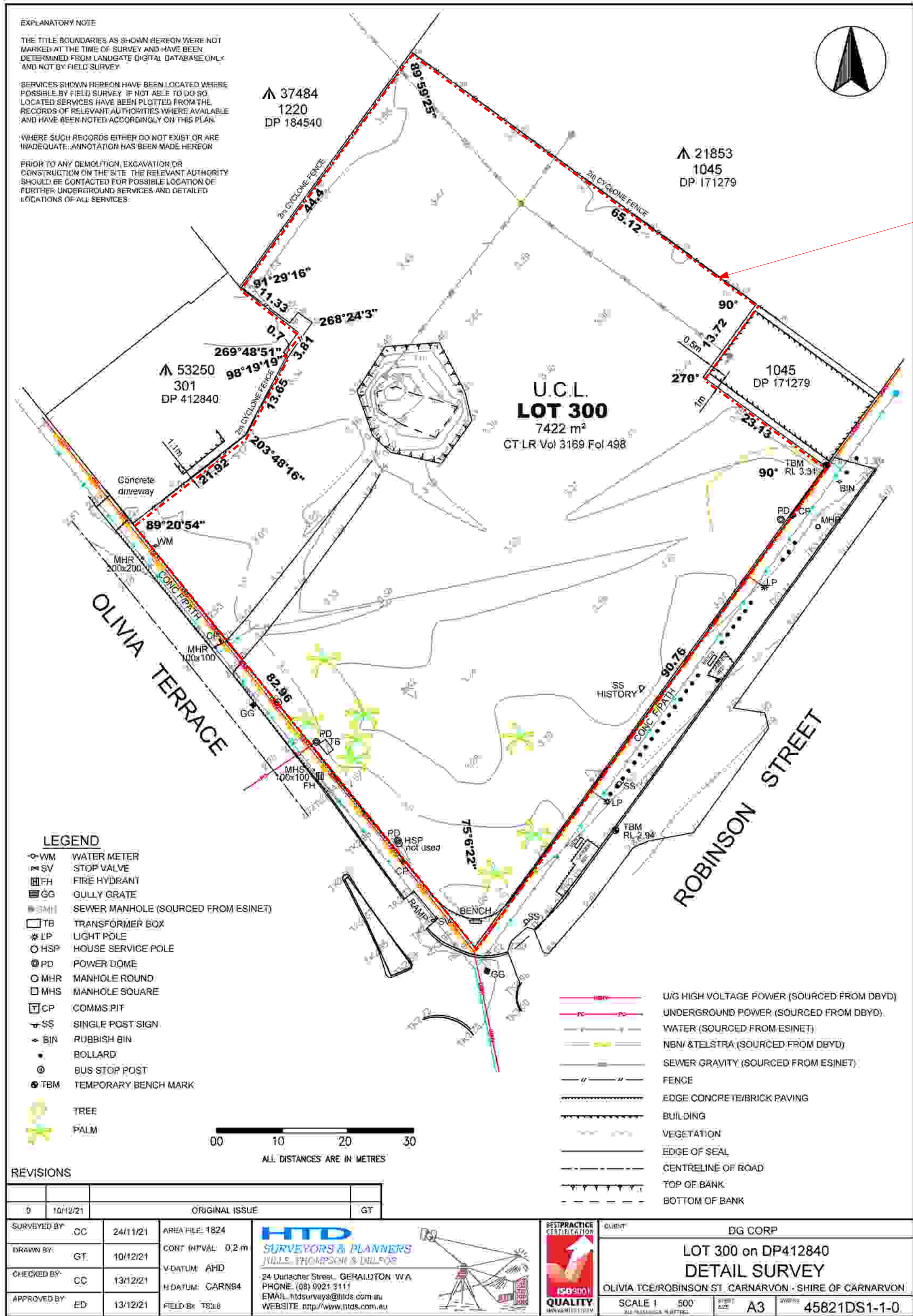


LEGEND:

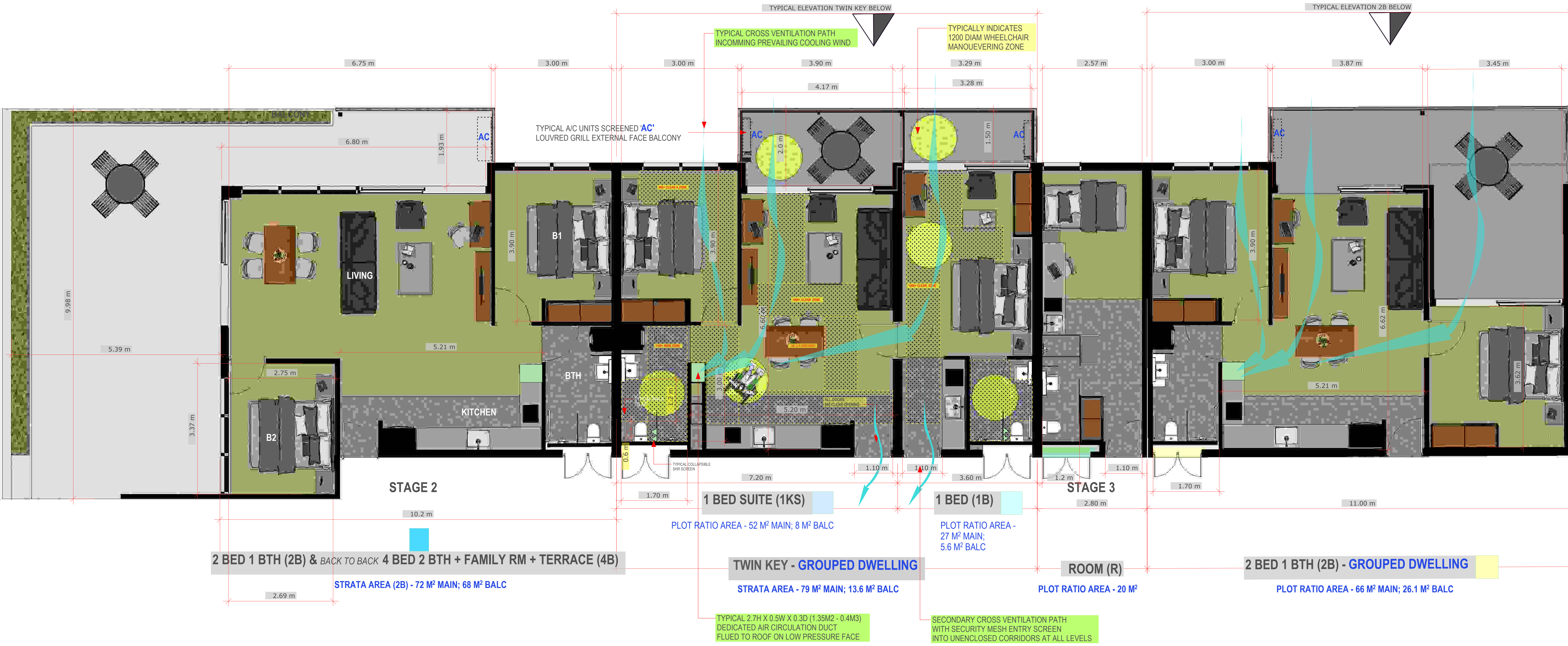
ABBREVIATIONS

- GF GROUND FLOOR
- FF FIRST FLOOR
- SF SECOND FLOOR
- TF THIRD FLOOR
- RL RELATIVE LEVEL BASED ON SURVEYOR'S DRAWINGS
- ALL LEVELS SHOWN ARE RLs
- FFL FINISHED FLOOR LEVEL
- FGL FINISHED GROUND LEVEL
- FPL FINISHED PAVING LEVEL

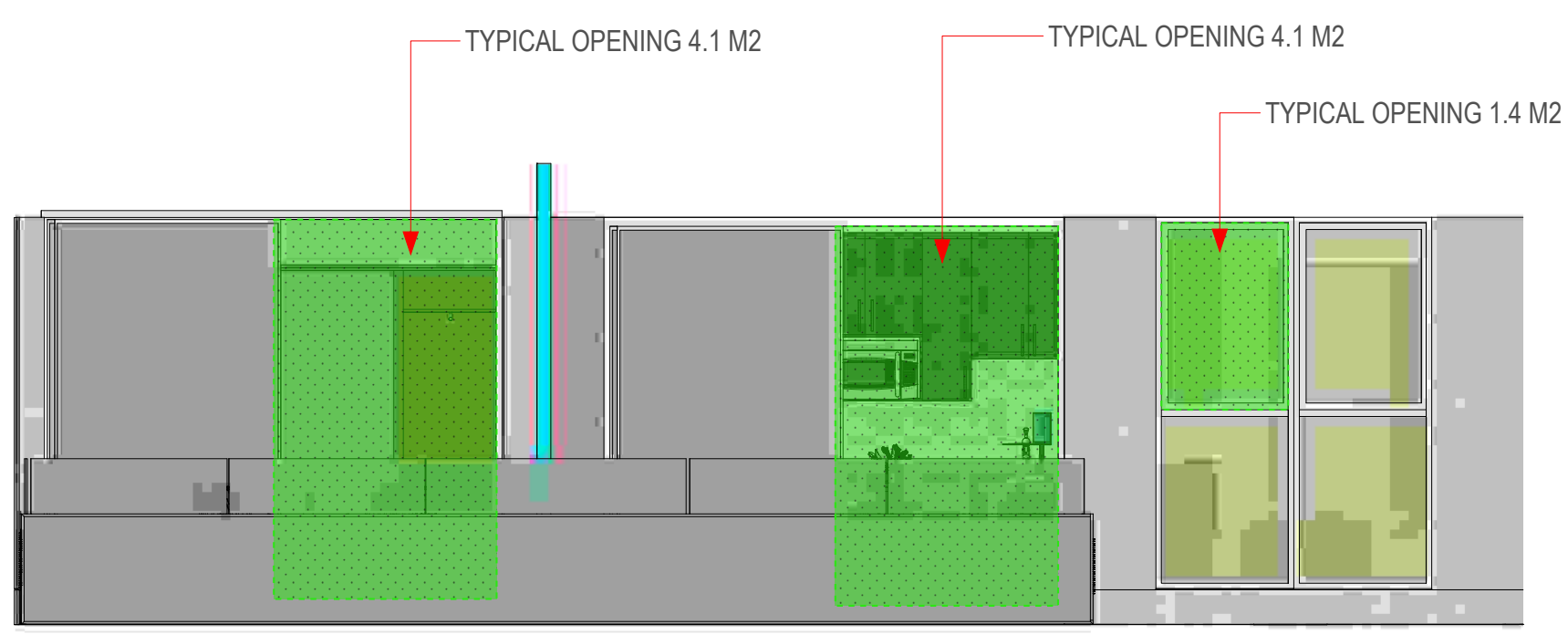
SITE SURVEY



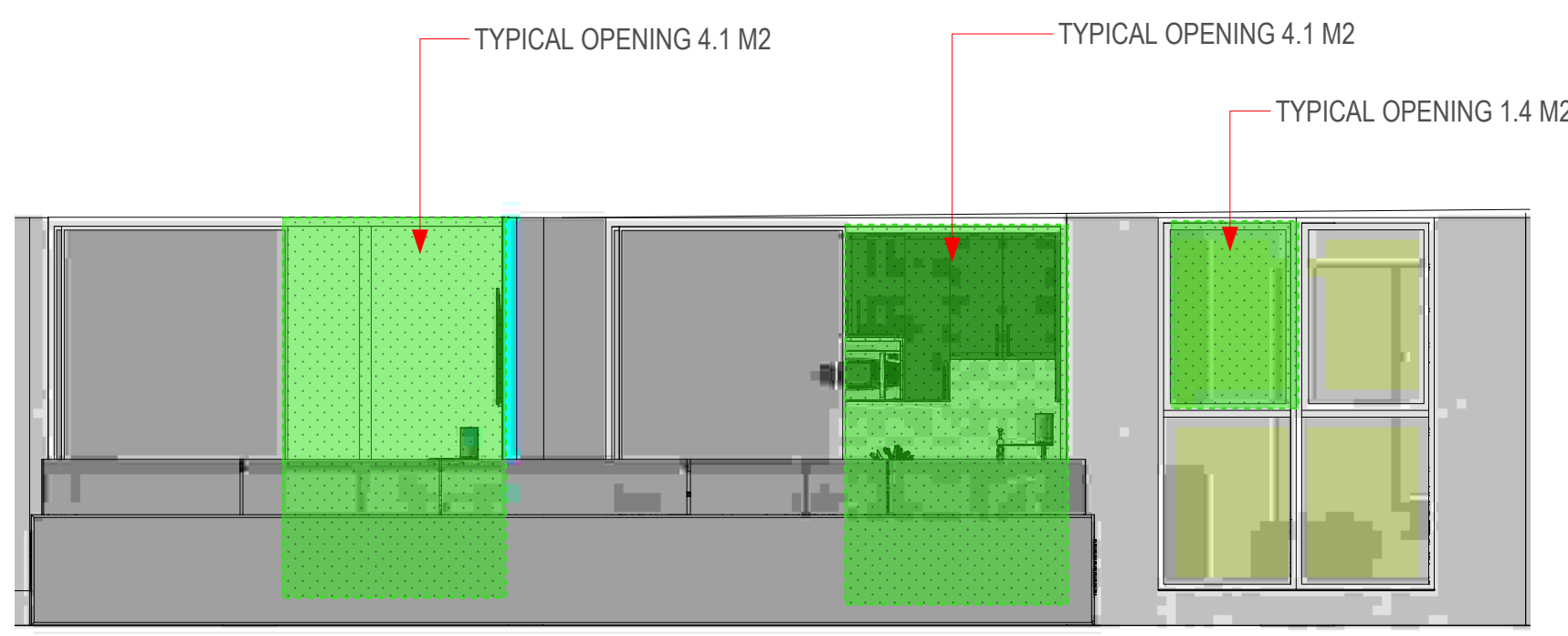




APARTMENTS & ROOMS TYPICAL PLANS



TYPICAL ELEVATION 2B GROUPED DWELLING



TYPICAL ELEVATION TWIN KEY GROUPED DWELLING

LIVEABLE HOUSING DESIGN GUIDELINES (LHDG)

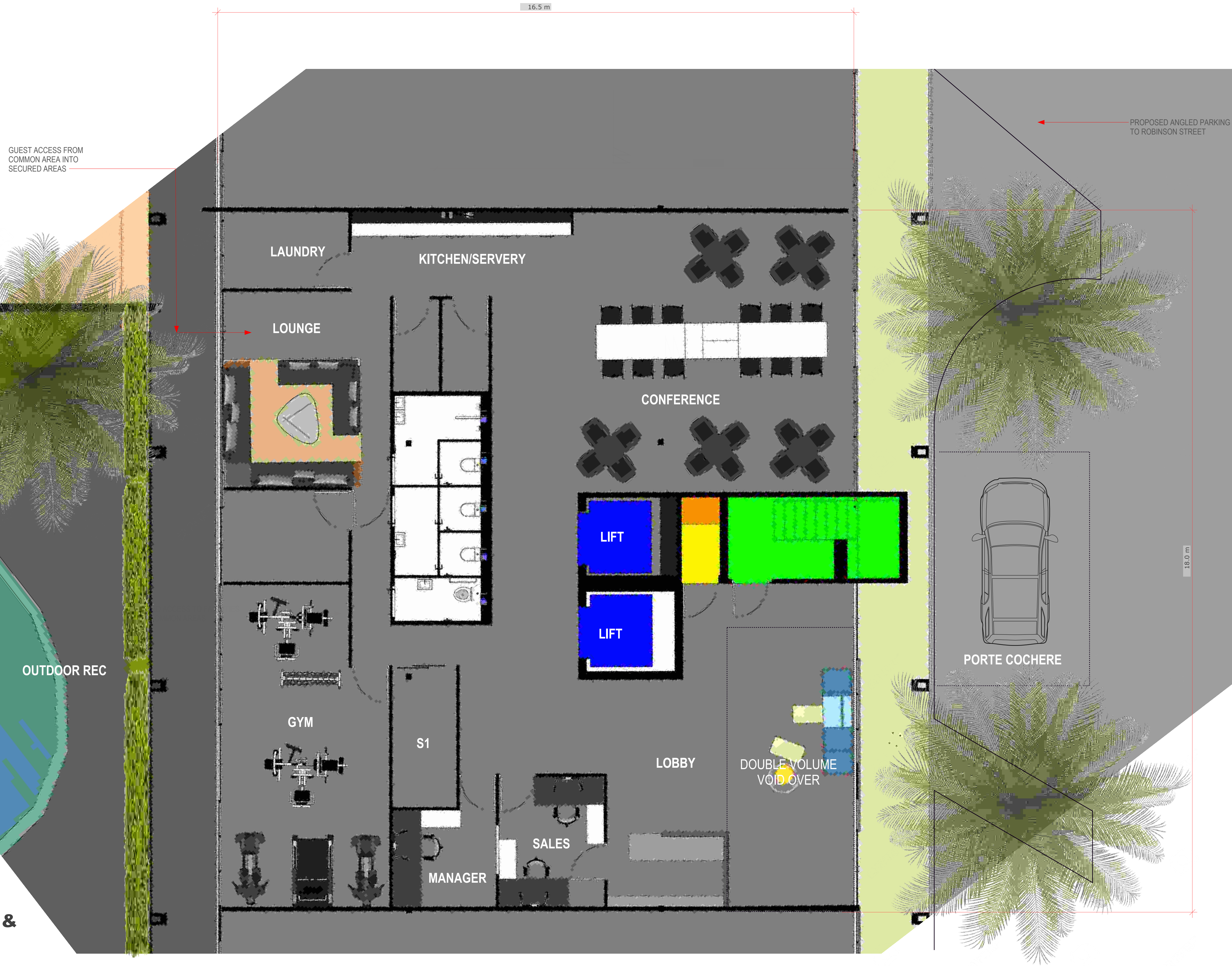
- ACCESSIBLE UNITS DESIGNATED ON FLOOR PLANS (DAU)
- ALL DOORS 900mm CLEAR OPENING
- ALL BATHROOMS ACCESSIBLE WITH GRAB RAILS
- COMPLIANT FIXTURES & BENCH HTS
- COLLAPSIBLE SHR SCREEN
- ALL DAU UNITS WITH COMPLIANT CONTINUOUS ACCESSIBLE PATH OF TRAVEL

MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE



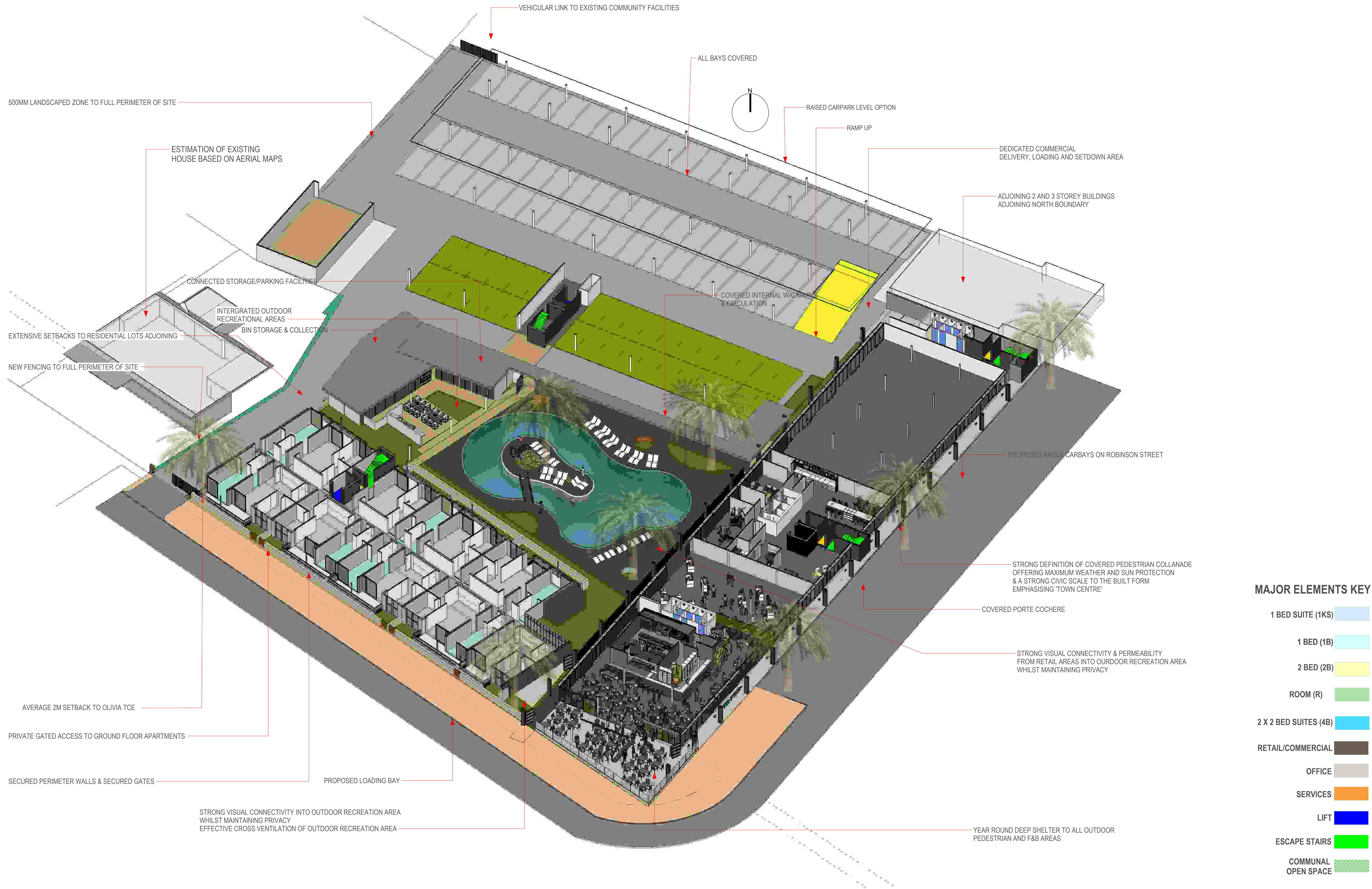
**HOTEL BOH & RECEPTION**



**MAJOR ELEMENTS KEY**

- 1 BED SUITE (1KS) [Light Blue Box]
- 1 BED (1B) [Light Cyan Box]
- 2 BED (2B) [Yellow Box]
- ROOM (R) [Light Green Box]
- 2 X 2 BED SUITES (4B) [Bright Blue Box]
- RETAIL/COMMERCIAL [Dark Brown Box]
- OFFICE [Light Grey Box]
- SERVICES [Orange Box]
- LIFT [Dark Blue Box]
- ESCAPE STAIRS [Bright Green Box]
- COMMUNAL OPEN SPACE [Dotted Green Box]





MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE



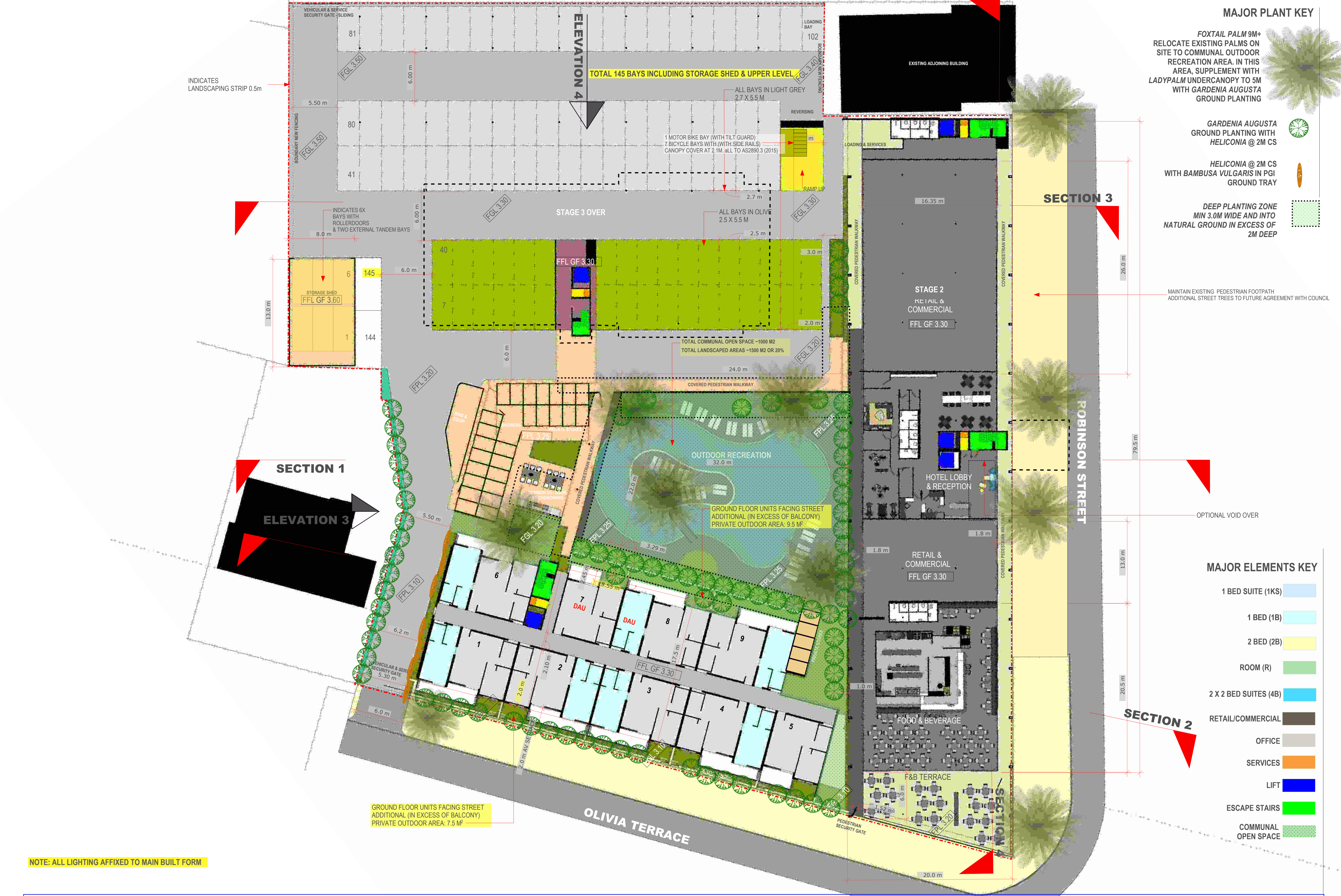
SCOPE OF PLANNING APPLICATION & BOUNDARY SHOWN BOUND IN RED  
BOUNDARY DIMENSION SHOWN IN WHITE (SURVEYORS DRAWING HAS PRECEDENCE)



MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS) [Light Blue Box]
- 1 BED (1B) [Cyan Box]
- 2 BED (2B) [Yellow Box]
- ROOM (R) [Green Box]
- 2 X 2 BED SUITES (4B) [Blue Box]
- RETAIL/COMMERCIAL [Brown Box]
- OFFICE [Grey Box]
- SERVICES [Orange Box]
- LIFT [Dark Blue Box]
- ESCAPE STAIRS [Red Box]
- COMMUNAL OPEN SPACE [Patterned Box]





November 3, 2022 | SCALE AT A1 SIZE

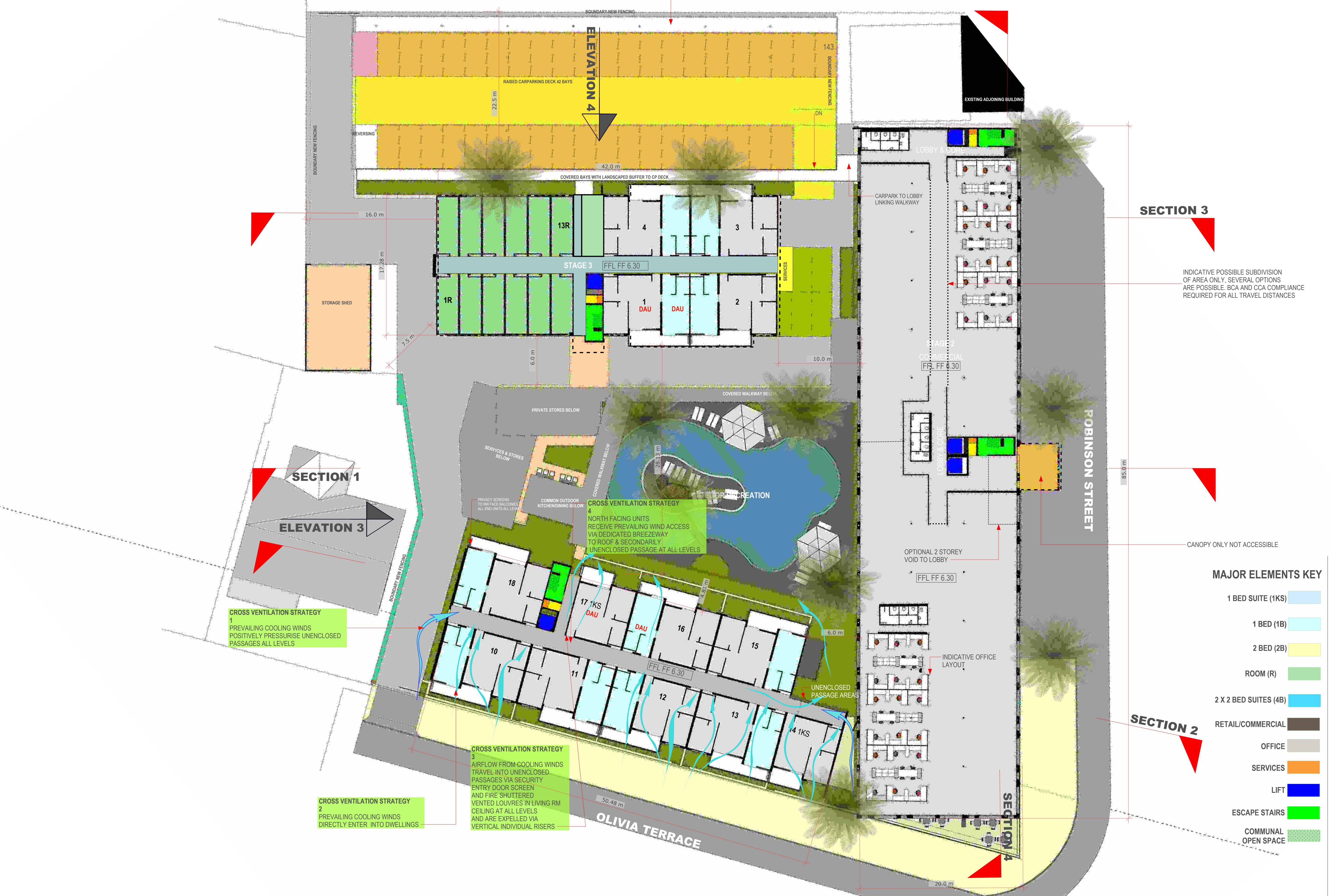
THE PIER CARNARVON - DG CORP - DEVELOPMENT APPLICATION

RESPONSE TO RF1 1 (SCHEDULE 1) ADDITIONAL CARPARK DECK

6 - GROUND FLOOR & SITE PLAN 1:200

ARCHITECTS PERRINE





SECTION 3

INDICATIVE POSSIBLE SUBDIVISION OF AREA ONLY. SEVERAL OPTIONS ARE POSSIBLE. BCA AND CCA COMPLIANCE REQUIRED FOR ALL TRAVEL DISTANCES

MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE

SECTION 2

ELEVATION 4

SECTION 1

ELEVATION 3

CROSS VENTILATION STRATEGY 4  
NORTH FACING UNITS RECEIVE PREVAILING WIND ACCESS VIA DEDICATED BREEZEWAY TO ROOF & SECONDARILY UNENCLOSED PASSAGE AT ALL LEVELS

CROSS VENTILATION STRATEGY 1  
PREVAILING COOLING WINDS POSITIVELY PRESSURISE UNENCLOSED PASSAGES ALL LEVELS

CROSS VENTILATION STRATEGY 3  
AIRFLOW FROM COOLING WINDS TRAVEL INTO UNENCLOSED PASSAGES VIA SECURITY ENTRY DOOR SCREEN AND FIRE SHUTTERED VENTED LOUVRES IN LIVING RM CEILING AT ALL LEVELS AND ARE EXPELLED VIA VERTICAL INDIVIDUAL RISERS

CROSS VENTILATION STRATEGY 2  
PREVAILING COOLING WINDS DIRECTLY ENTER INTO DWELLINGS









SECTION 3

SECTION 4

SECTION 1

ELEVATION 3

SECTION 2

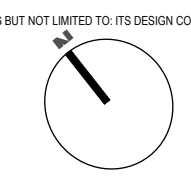
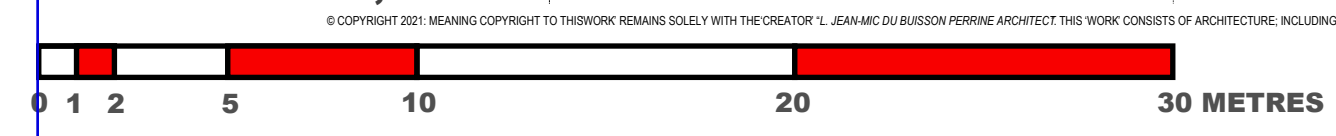
MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE

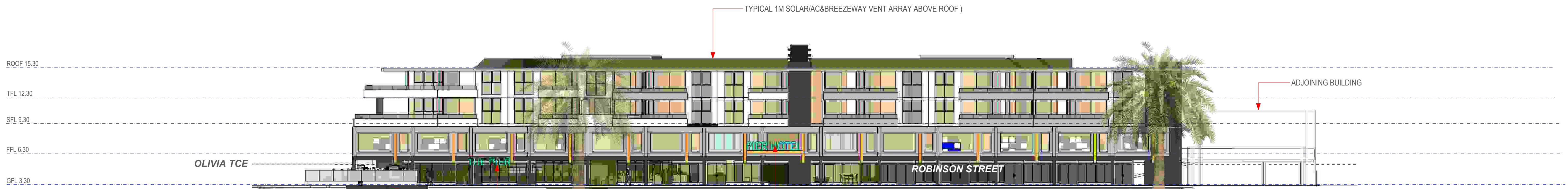
November 3, 2022 | SCALE AT A1 SIZE | 1:200

THE PIER CARNARVON - DG CORP - DEVELOPMENT APPLICATION | RESPONSE TO RFI 1 (SCHEDULE 1) ADDITIONAL CARPARK DECK

9 - THIRD FLOOR 1:200 | ARCHITECTS PERRINE







**ROBINSON STREET ELEVATION**

TYPICAL DESIGNATED  
SIGNAGE ZONE  
BETWEEN EACH VERTICAL  
FIN (ORANGE)

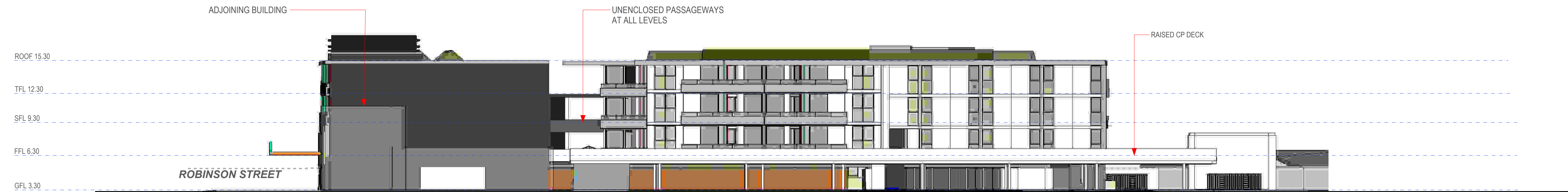
PROPOSED MAJOR HOTEL SIGNAGE  
AS SHOWN



**OLIVIA STREET ELEVATION**



**ELEVATION 3**



**ELEVATION 4**

**MAJOR ELEMENTS KEY**

- 1 BED SUITE (1KS) [Light Blue Box]
- 1 BED (1B) [Light Cyan Box]
- 2 BED (2B) [Yellow Box]
- ROOM (R) [Green Box]
- 2 X 2 BED SUITES (4B) [Blue Box]
- RETAIL/COMMERCIAL [Dark Brown Box]
- OFFICE [Light Brown Box]
- SERVICES [Orange Box]
- LIFT [Dark Blue Box]
- ESCAPE STAIRS [Bright Green Box]
- COMMUNAL OPEN SPACE [Patterned Green Box]

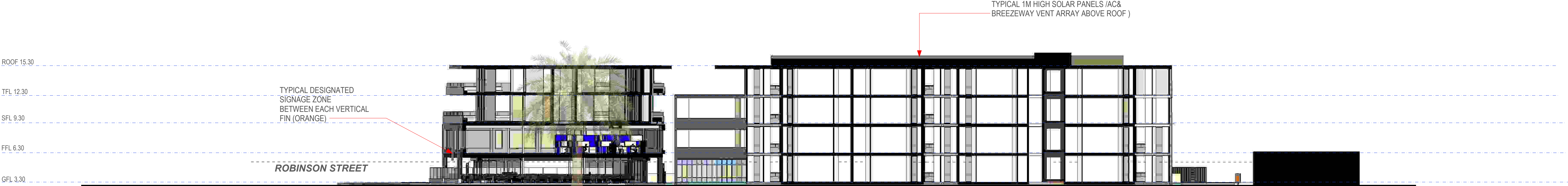




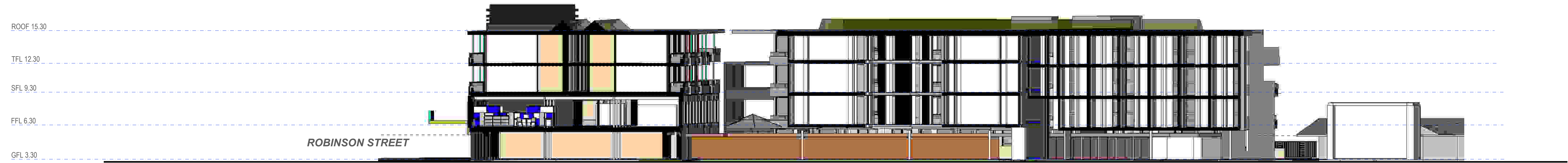
# MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE

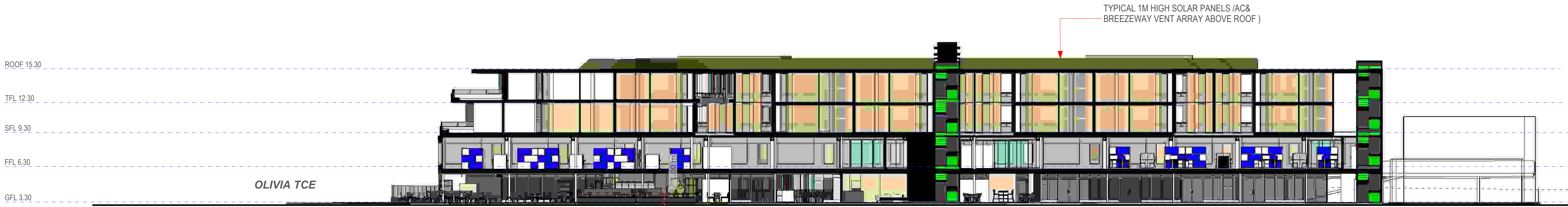




SECTION 2



SECTION 3



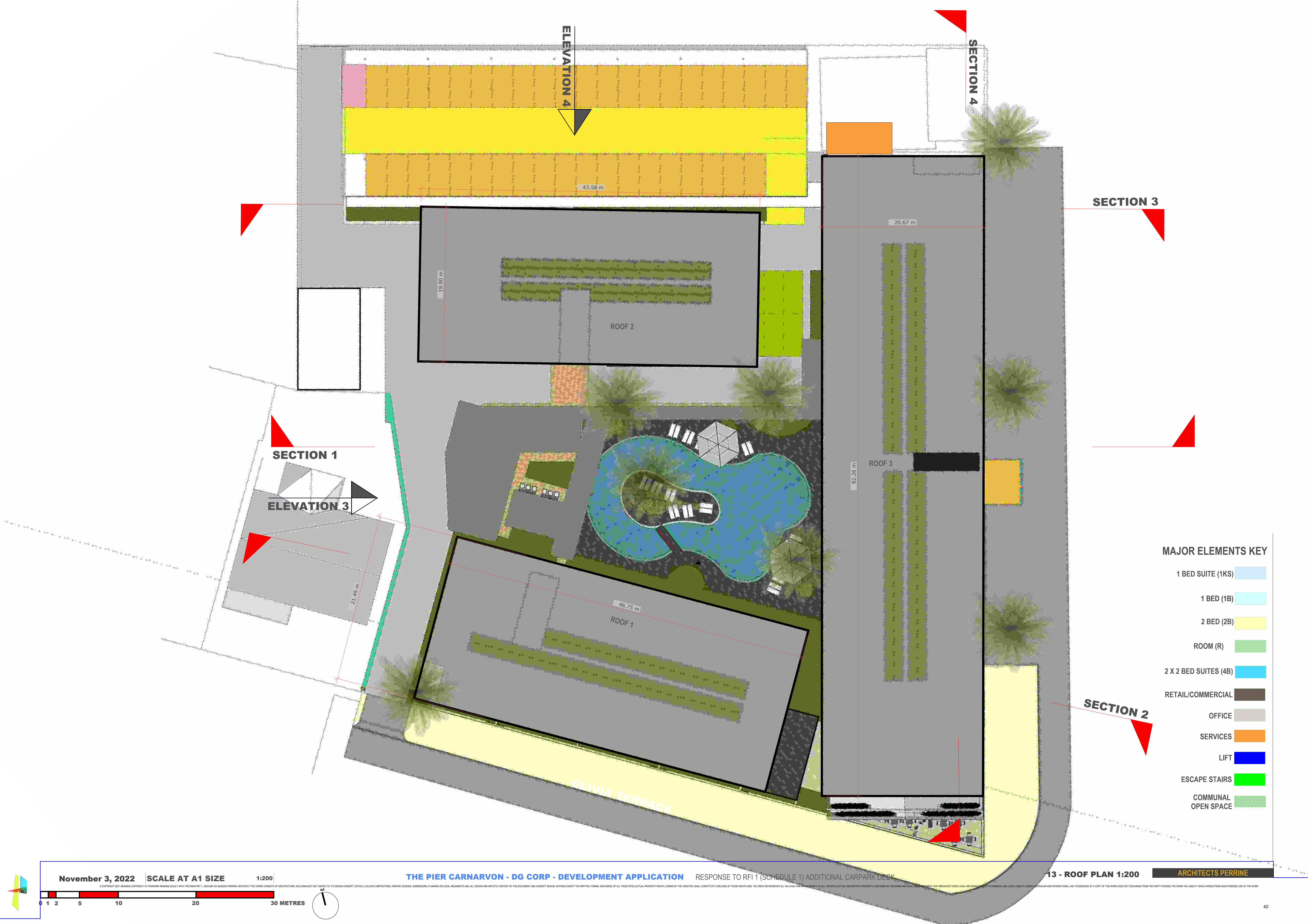
SECTION 4

ROBINSON STREET

MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE





MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE

November 3, 2022

SCALE AT A1 SIZE

1:200

THE PIER CARNARVON - DG CORP - DEVELOPMENT APPLICATION

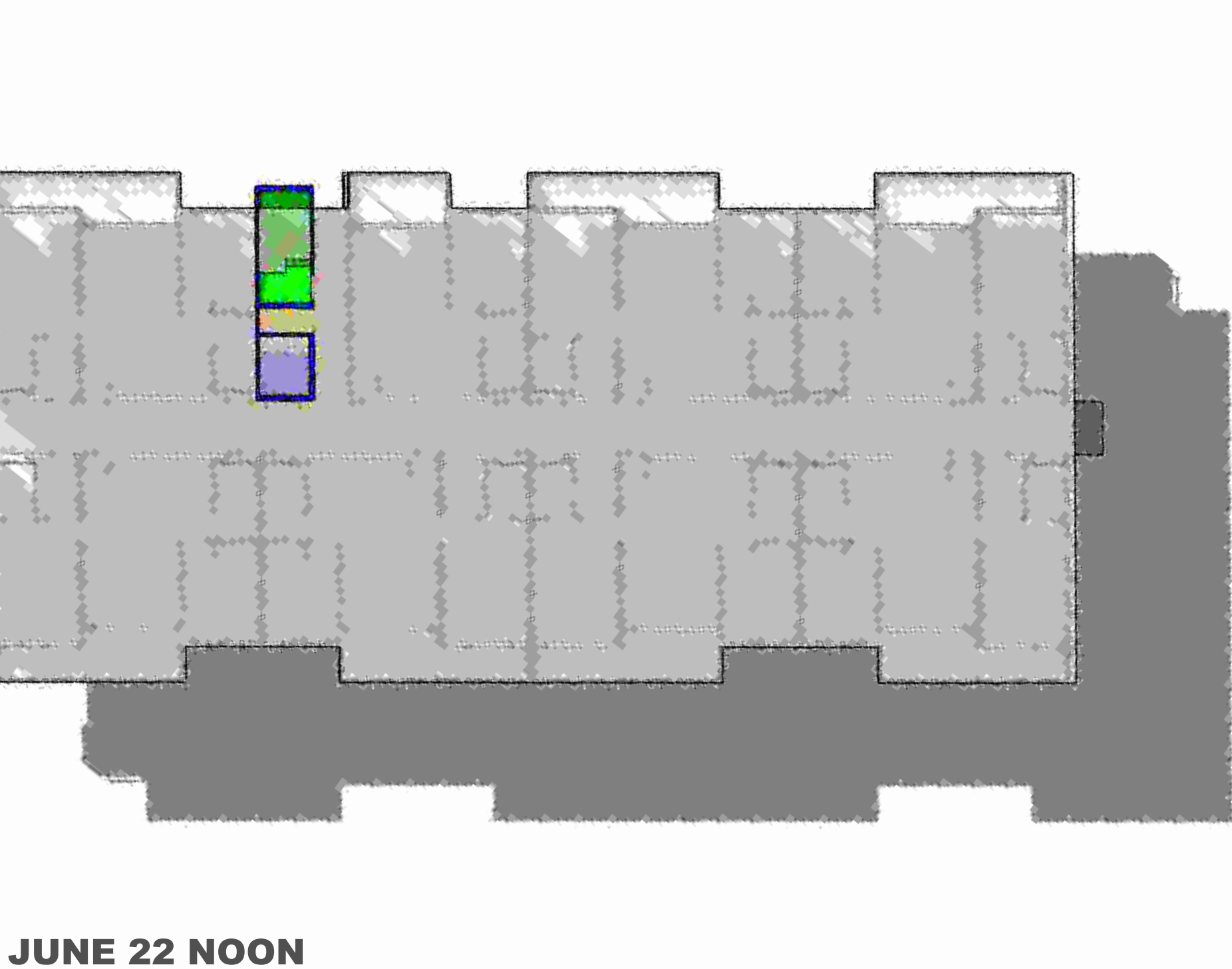
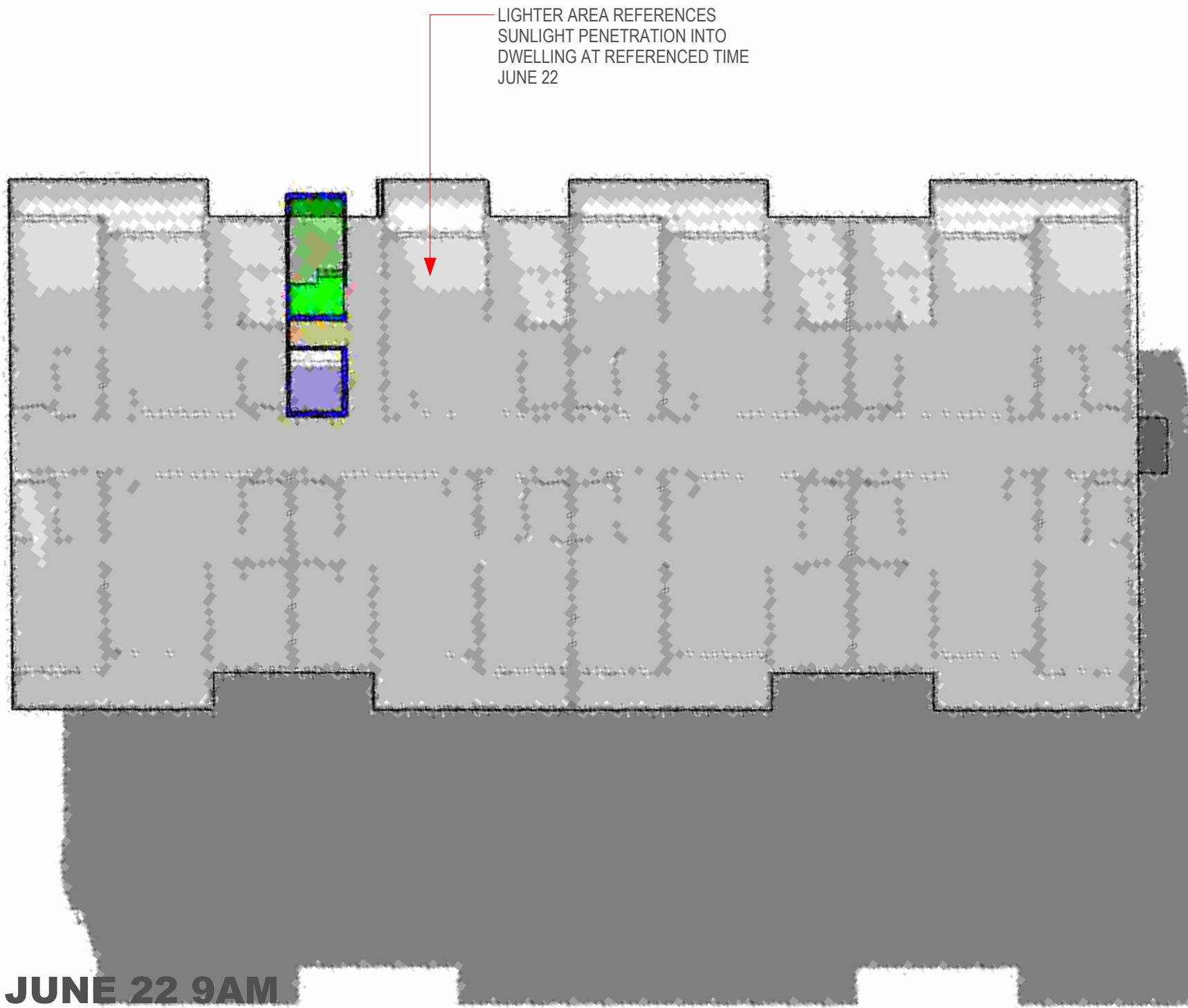
RESPONSE TO RFI 1 (SCHEDULE 1) ADDITIONAL CARPARK DECK

13 - ROOF PLAN 1:200

ARCHITECTS PERRINE

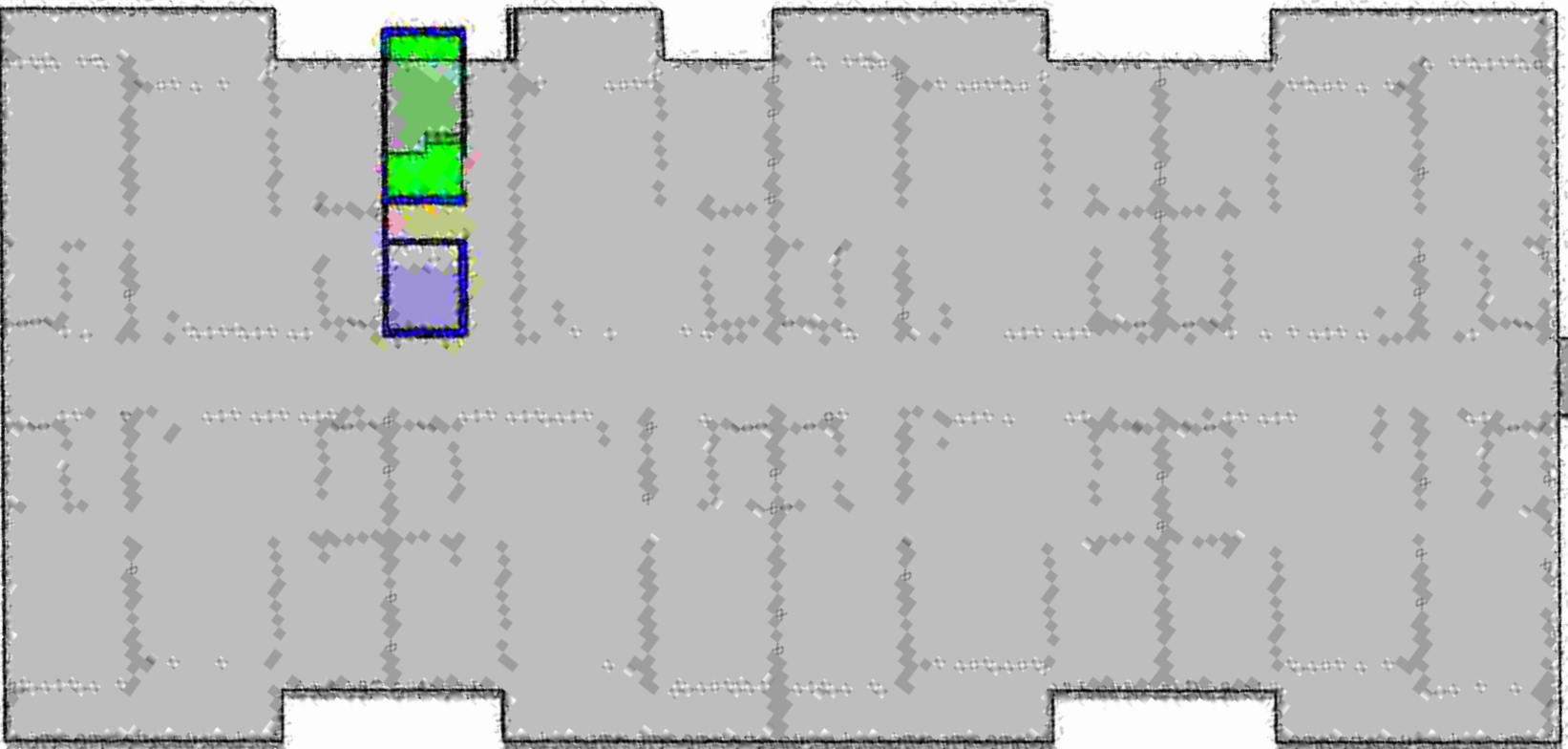
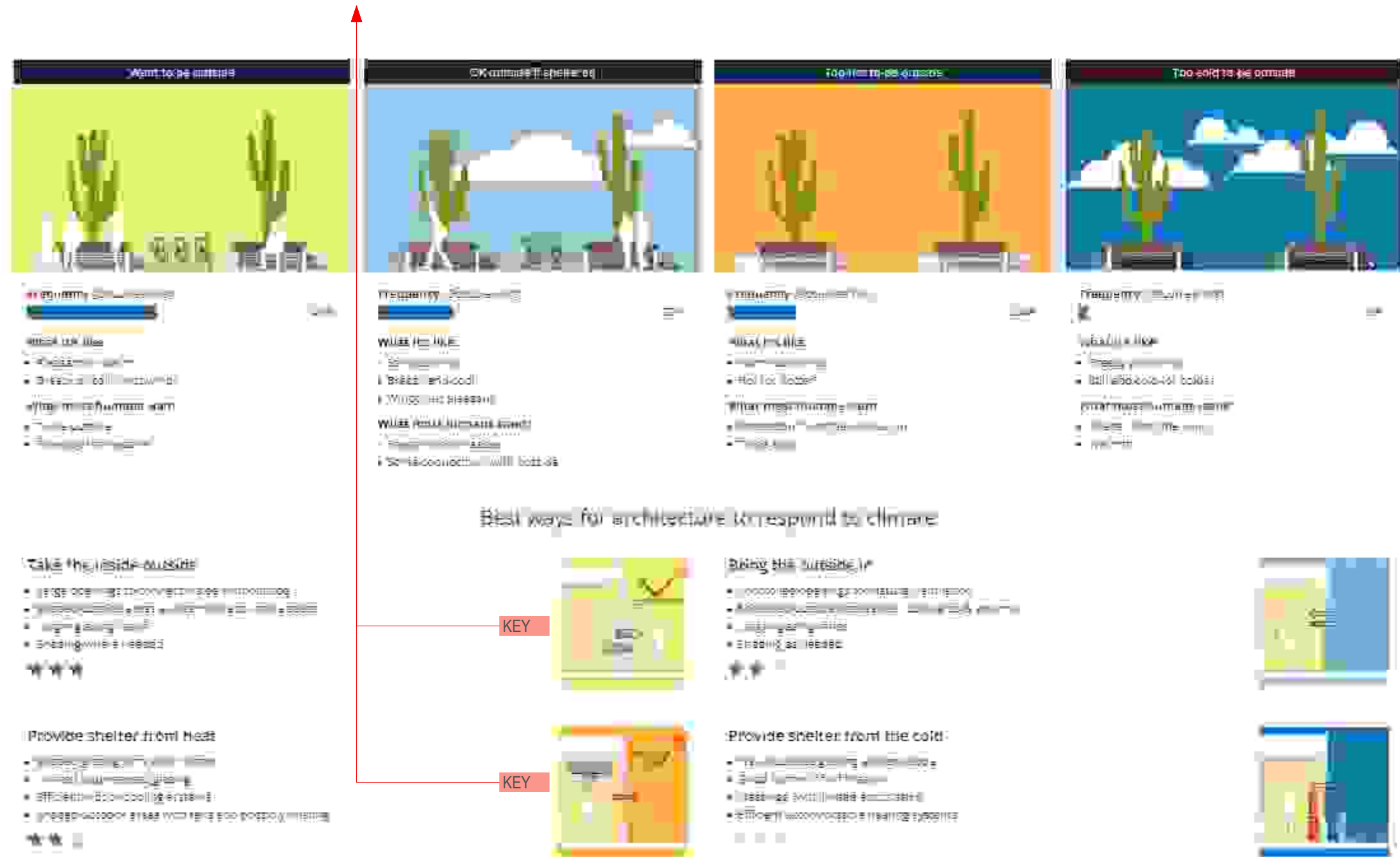
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### JUNE 22 PENETRATION OF SUNLIGHT DIAGRAM

UNLIKE PERTH, MINIMISATION OF DIRECT SUN PENETRATION INTO DWELLINGS IS KEY TO SUSTAINABLE CLIMATIC CONTROL



### MAJOR ELEMENTS KEY

- 1 BED SUITE (1KS)
- 1 BED (1B)
- 2 BED (2B)
- ROOM (R)
- 2 X 2 BED SUITES (4B)
- RETAIL/COMMERCIAL
- OFFICE
- SERVICES
- LIFT
- ESCAPE STAIRS
- COMMUNAL OPEN SPACE

## Car Parking Assessment A

### (A) Car Parking Requirements - Calculations:

#	Use Class		Formula	Bays	
				Required	Proposed
<b>1</b>	<b>RESTAURANT/CAFÉ (196 seats)</b>		<b>1 bay per 4 seated persons</b>		
	(i)	Internal	$116/4 = 29$ bays	29	Not designated
	(ii)	Terrace	$80/4 = 20$ bays	20	Not designated
		<b>Total</b>	<b>Seats - 196</b>	<b>49</b>	<b>?</b>
<b>2</b>	<b>SHOP (2 Units)</b>		<b>1 bay per 20 m<sup>2</sup> nett lettable area (NLA)</b>		
	(i)	Ground floor – west of Hotel Lobby	$NLA \sim 213 \text{ m}^2/20\text{m}^2 = 10.65$ bays	11	Not designated
	(ii)	Ground floor – east of Hotel Lobby	$NLA \sim 425 \text{ m}^2/20\text{m}^2 = 21.25$ bays	22	Not designated
		<b>Total</b>		<b>33</b>	<b>?</b>
<b>3</b>	<b>OFFICE (COMMERCIAL)</b>		<b>1 bay per 50 m<sup>2</sup> nett lettable area (NLA)</b>		
	(i)	First floor	$NLA \sim 1,380 \text{ m}^2/50\text{m}^2 = 27.6$ bays	28	Not designated
		<b>Total</b>		<b>28</b>	<b>?</b>
<b>4</b>	<b>HOTEL (49 suites)</b>		<b>1 bay per bedroom/unit + 1 bay per 15 m<sup>2</sup> NLA (excl. accommodation) + 3 car queuing bays</b>		
	(i)	Second floor suites	25 units (King-bed suites + Double-bed suites)	25	Not designated
	(ii)	Third floor suites	24 units (King-bed suites + Double-bed suites)	24	Not designated
	(iii)	NLA - none		-	
	(iv)	Porte Cochere		<b>3</b>	<b>1</b>
		<b>Total</b>		<b>49</b>	<b>?</b>
<b>5</b>	<b>SHORT-STAY SUITES (24 suites)</b>		<b>1 bay per bedroom/unit + 1 bay per 15 m<sup>2</sup> NLA (excl. accommodation) + 3 car queuing bays</b>		
	(i)	First floor suites	8 units (King-bed suites + Double-bed suites)	8	Not designated
	(ii)	Second floor suites	8 units (King-bed suites + Double-bed suites)	8	Not designated
	(iii)	Third floor suites	8 units (King-bed suites + Double-bed suites)	8	Not designated
		<b>Total</b>		<b>24</b>	<b>?</b>

#	Use Class		Formula	Bays	
				Required	Proposed
6	MULTIPLE DWELLINGS (61 dwellings)		Location B under R-Codes:		
	(i)	Ground floor	1 bay x 16 one-bedroom (KS + S) units	16	Not designated
	(ii)	First floor	1 bay x 16 one-bedroom (KS +S) units	16	Not designated
	(iii)	Second floor	1 bay x 14 one-bedroom (KS + S) units	14	Not designated
			1.25 bays x 1 two-bedroom (2B) unit	2	Not designated
	(v)	Third floor	1 bay x 13 one-bedroom (KS + S) units	13	Not designated
			1.25 bays x 1 two-bedroom (2B)	2	Not designated
	(vi)	Visitor bays	1 bay x 4 dwellings (first 12 dwellings)	3	Not designated
			1 bay x 8 dwellings (for remaining 49 dwellings)	7	Not designated
	Sub-total		61 dwellings	73	
	Applicable deduction		Number of onsite car bays required can be reduced for every 5 motorcycle or scooter bays proposed – only 1 motorcycle bay proposed so reduction not applicable.		Nil
7	STUDIO SUITES (39 suites)		To be determined by the Council – say, 1 bay per dwelling unit		
		First floor	13 units	13	Not designated
		Second floor	13 units	13	Not designated
		Third floor	13 units	13	Not designated
	Total			39	

<b>TOTALS</b>	<b>295</b>	<b>143<sup>^</sup></b>
---------------	------------	------------------------

### Calculated onsite parking bays shortfall – 152 bays

<sup>^</sup> Does not include the Porte Cochere which will displace at least 3 public parking bays in Robinson Street.

Neither has any allowance been made for reciprocal parking utilising both proposed onsite car bays and existing public car bays within Robinson Street and the Fascine waterway foreshore car park in Olivia Terrace.

### (B) Bicycle Parking Requirements - Calculations:

#	Use Class		Formula	Bays	
				Required	Proposed
<b>1</b>	<b>MULTIPLE DWELLINGS (61)</b>		<b>Location B under R-Codes:</b>		
	(i)	Resident	0.5 space x 61 dwellings	31	35*
	(ii)	Visitor	1 space x 10 dwellings	6	7
	<b>Total</b>			<b>37</b>	<b>42</b>
*	A dedicated bicycle space is proposed within each storage area proposed for 35 of the multiple dwellings				

**(C) Motorcycle/Scooter Parking Requirements - Calculations:**

#	Use Class		Formula	Bays	
				Required	Proposed
<b>1</b>	<b>MULTIPLE DWELLINGS (61)</b>		<b>Location B under R-Codes:</b>		
	Development exceeds 20 dwellings		1 space for every 10 car bays (6 bays required based on the calculated requirement of 61 car bays)	6	1
	<b>Total</b>			<b>6</b>	<b>1</b>

\*\*\*

## Car Parking Assessment B

### (A) Car Parking Requirements - Calculations:

#	Use Class		Formula	Bays	
				Required	Proposed
<b>1</b>	<b>RESTAURANT/CAFÉ (196 seats)</b>		<b>1 bay per 4 seated persons</b>		
	(i)	Internal	$116/4 = 29$ bays	29	Not designated
	(ii)	Terrace	$80/4 = 20$ bays	20	Not designated
		<b>Total</b>	<b>Seats - 196</b>	<b>49</b>	<b>?</b>
<b>2</b>	<b>SHOP (2 Units)</b>		<b>1 bay per 20 m<sup>2</sup> nett lettable area (NLA)</b>		
	(i)	Ground floor – west of Hotel Lobby	$NLA \sim 213 \text{ m}^2/20\text{m}^2 = 10.65$ bays	11	Not designated
	(ii)	Ground floor – east of Hotel Lobby	$NLA \sim 425 \text{ m}^2/20\text{m}^2 = 21.25$ bays	22	Not designated
		<b>Total</b>		<b>33</b>	<b>?</b>
<b>3</b>	<b>OFFICE (COMMERCIAL)</b>		<b>1 bay per 50 m<sup>2</sup> nett lettable area (NLA)</b>		
	(i)	First floor	$NLA \sim 1,380 \text{ m}^2/50\text{m}^2 = 27.6$ bays	28	Not designated
		<b>Total</b>		<b>28</b>	<b>?</b>
<b>4</b>	<b>HOTEL (49 suites)</b>		<b>1 bay per bedroom/unit + 1 bay per 15 m<sup>2</sup> NLA (excl. accommodation) + 3 car queuing bays</b>		
	(i)	Second floor suites	25 units (King-bed suites + Double-bed suites)	25	Not designated
	(ii)	Third floor suites	24 units (King-bed suites + Double-bed suites)	24	Not designated
	(iii)	NLA - none		-	
	(iv)	Porte Cochere		3	1
		<b>Total</b>		<b>49</b>	<b>?</b>
<b>5</b>	<b>SHORT-STAY SUITES (24 Suites)</b>		<b>1 bay per bedroom/unit + 1 bay per 15 m<sup>2</sup> NLA (excl. accommodation) + 3 car queuing bays</b>		
	(i)	First floor suites	8 units (King-bed suites + Double-bed suites)	8	Not designated
	(ii)	Second floor suites	8 units (King-bed suites + Double-bed suites)	8	Not designated
	(iii)	Third floor suites	8 units (King-bed suites + Double-bed suites)	8	Not designated
		<b>Total</b>		<b>24</b>	<b>?</b>



#	Use Class		Formula	Bays	
				Required	Proposed
6	MULTIPLE DWELLINGS (61 dwellings)		Location B under R-Codes:		
	(i)	Ground floor	1 bay x 2 one-bedroom (KS) units	2	Not designated
			1.25 bays x 7 two-bedroom ('twin-key' units)	9	Not designated
	(ii)	First floor	1 bay x 2 one-bedroom (KS) units	2	Not designated
			1.25 bays x 7 two-bedroom ('twin-key' units)	9	Not designated
	(iii)	Second floor	1 bay x 2 one-bedroom (KS) units	2	Not designated
			1.25 bays x 7 two-bedroom (which 6 are 'twin-key' units)	9	Not designated
	(v)	Third floor	1 bay x 1 one-bedroom (KS) units	1	Not designated
			1.25 bays x 7 two-bedroom (which 6 are 'twin-key' units)	9	Not designated
	(vi)	Visitor bays	1 bay x 4 dwellings (first 12 dwellings)	3	Not designated
			1 bay x 8 dwellings (for remaining 23 dwellings)	3	Not designated
	Sub-total		35 dwellings	49	
	Applicable deduction		Number of onsite car bays required can be reduced for every 5 motorcycle or scooter bays proposed – only 1 motorcycle bay proposed so reduction not applicable.		Nil
7	STUDIO SUITES (39 suites)		To be determined by the Council – say, 1 bay per dwelling unit		
		First floor	13 units	13	Not designated
		Second floor	13 units	13	Not designated
		Third floor	13 units	13	Not designated
	Total			39	
TOTALS				271	143^

**Calculated onsite parking bays shortfall – 128 bays**

<sup>^</sup> Does not include the Porte Cochere which will displace at least 3 public parking bays in Robinson Street.

Neither has any allowance been made for reciprocal parking utilising both proposed onsite car bays and existing public car bays within Robinson Street and the Fascine waterway foreshore car park in Olivia Terrace.



**(B) Bicycle Parking Requirements - Calculations:**

#	Use Class		Formula	Bays	
				Required	Proposed
<b>1</b>	<b>MULTIPLE DWELLINGS (59)</b>		<b>Location B under R-Codes:</b>		
	(i)	Resident	0.5 space x 35 dwellings	18	35*
	(ii)	Visitor	1 space x 10 dwellings	4	7
	<b>Total</b>			<b>22</b>	<b>42</b>
*	A dedicated bicycle space is proposed within each storage area proposed for each multiple dwelling				

**(C) Motorcycle/Scooter Parking Requirements - Calculations:**

#	Use Class		Formula	Bays	
				Required	Proposed
<b>1</b>	<b>MULTIPLE DWELLINGS (35)</b>		<b>Location B under R-Codes:</b>		
	Development exceeds 20 dwellings		1 space for every 10 car bays (4.9 bays required based on the applicant's calculated requirement of 49 car bays)	5	1
	<b>Total</b>			<b>5</b>	<b>1</b>

\*\*\*

# Technical Note

Subject:	Proposed Mixed-use Development – 85 Olivia Terrace, Carnarvon Parking Demand Assessment (2210020-TN-001)		
Date:	10 <sup>th</sup> November 2022		
Author:	P. Nguyen	Reviewer:	R. Needham
Client:	DG Corporation		

## 1. Introduction

DG Corporation are proposing a mixed-use development to be located at 85 Olivia Terrace in Carnarvon.

The proposed development comprises 35 residential apartments, a hotel with up to 113 total accommodation units (including 68 linkable units), office floorspace, retail / commercial floorspace and a restaurant / café.

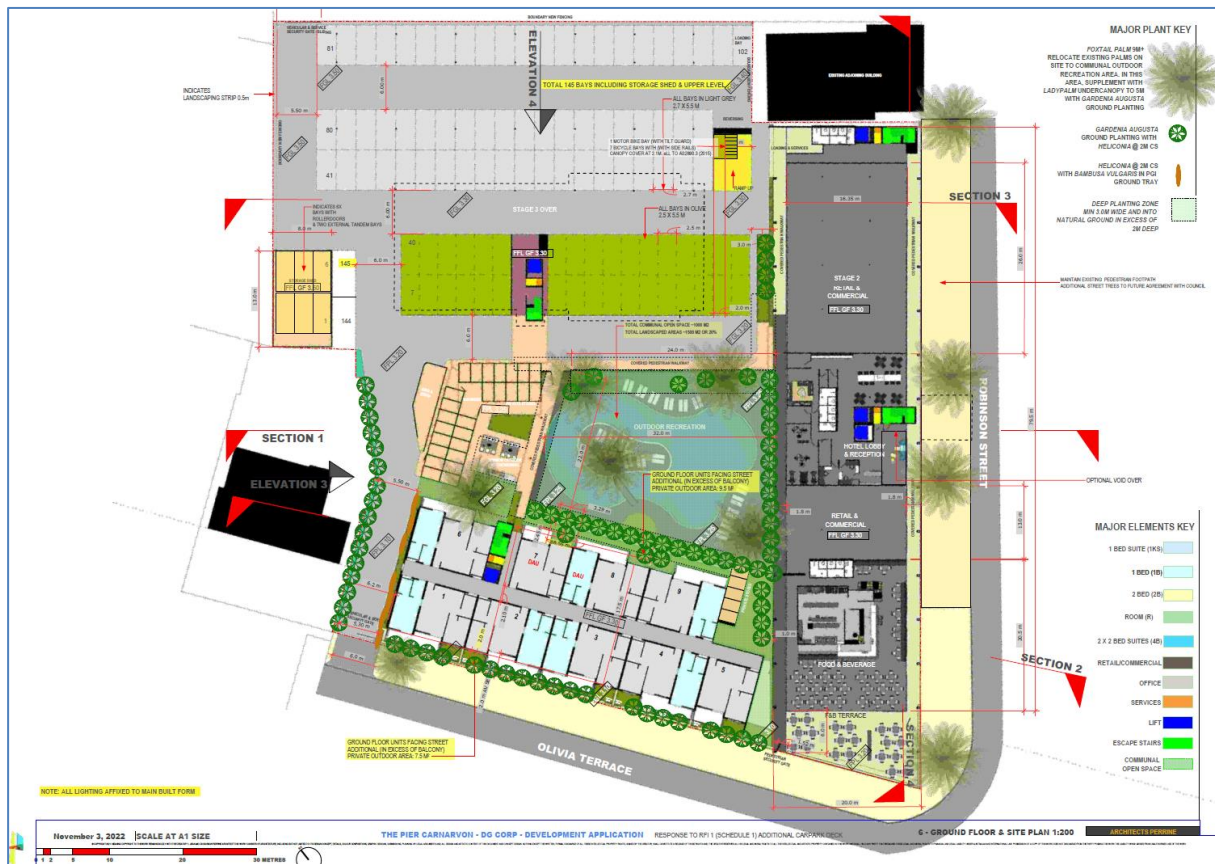
A car park with approximately 145 car parking bays is also proposed on the site. Three of the existing car parking bays along Robinson Street will be replaced by a porte cochere with three pick-up and drop-off spaces.

The Shire of Carnarvon have requested a parking demand study in order to determine the actual parking demand of the proposed development. The existing site is shown in **Figure 1**.



Figure 1: Existing Site

The site layout and ground floor plan is shown in **Figure 2**.



**Figure 2: Site Layout and Ground Floor Plan**

## 2. Review

### 2.1. Parking Requirements

The car parking requirements for development in this area are outlined in the Shire of Carnarvon's Local Planning Scheme No 13 (LPS13). For residential development, the Residential Design Codes Volume 2 – Apartments (RDC2) are applicable. Based on the location of the site, the Location B requirements of RDC2 have been used.

As many of the hotel rooms/apartments will be dual-key units that can be booked separately or together, the number of accommodation units may vary from 79 total units to 113 total units. For the purposes of this assessment, the typical number of accommodation units is taken to be the average of the above range which is 96 units.

The car parking requirements for the development are calculated in **Table 1** and **Table 2**.

**Table 1: Car Parking Requirements - Residential**

Land Use	Requirement	Quantum	Required Bays
Residential (Apartments)	1 bay per 1 bedroom dwelling	7	7
	1.25 bay per 2+ bedroom dwelling	28	35
Residential Visitor	1 bay per 4 dwellings up to 12 dwellings	12	3
	1 bay per 8 dwellings for the 13 <sup>th</sup> dwelling and above	23	3
			42 resident bays 6 visitor bays

**Table 2: Car Parking Requirements – Non-Residential**

Land Use	Requirement	Quantum	Required Bays
Hotel	1 bay per bedroom or accommodation units	96 units	96
	1 bay per 15m <sup>2</sup> NLA of non-accommodation floorspace	None	0
	3 car queuing bays	Porte cochere	3
Office	1 bay per 50m <sup>2</sup> NLA	1,125m <sup>2</sup> NLA	23
Retail / Shop	1 bay per 20m <sup>2</sup> NLA in the Regional Centre Zone	637m <sup>2</sup> NLA	32
Restaurant / Café	1 bay per 4 seated persons	196 persons	28
			182

As the peak periods of parking demand for residential visitors (mostly weeknights and on weekends) do not fully overlap with the peak periods of demand for the commercial uses, it is considered that the 6 residential visitor bays can be included in the commercial bays under a reciprocal arrangement.

As above, the development requires a minimum of 224 car parking bays including 42 resident bays and 182 commercial bays.

## 2.2. Parking Demand Assessment

Clause 31.7 of LPS13 allows car parking spaces to be provided jointly by two or more users of land or one owner or user of land in respect of separate building or uses where the peak hours of operation of the buildings or uses sharing such parking are different and do not substantially overlap.

Based on the mix of uses within the proposed development, it is likely that the peak hours of parking demand do not substantially overlap and there is potential for parking to be shared between the differing uses. A parking demand assessment has therefore been undertaken to estimate the overall peak parking demand of the non-residential uses. The typical parking demand profile for the proposed uses has been obtained from the Institute of Transportation Engineers *Parking Generation*, 5<sup>th</sup> Edition. *Parking Generation* provides information on parking demands for various land uses based on surveys at existing developments including:

- The typical periods of peak parking demand
- The average peak parking demand
- For some uses, a parking demand profile detailing the percentage of the peak parking demand throughout the day.

The parking demand profile for the non-residential uses are shown in **Figure 3** to **Figure 7**. The following land uses were applied:

- *All Suites Hotel* was used for the hotel component
- *General Office Building* was used for the office component
- *Supermarket* was used for the retail component
- *High-Turnover (Sit-Down) Restaurant* was used for the restaurant / café tenancy. For this use, the parking demand profile for the “Family - Lunch and Dinner” restaurant was used as this was the closest match to the proposed use as advised by the client.



Land Use: 311 All Suites Hotel		
Hour Beginning	Percent of Peak Parking Demand	
	Weekday	Saturday
12:00–4:00 a.m.	100	100
5:00 a.m.	–	–
6:00 a.m.	89	76
7:00 a.m.	81	77
8:00 a.m.	72	74
9:00 a.m.	71	69
10:00 a.m.	48	50
11:00 a.m.	41	45
12:00 p.m.	29	35
1:00 p.m.	30	33
2:00 p.m.	25	34
3:00 p.m.	34	40
4:00 p.m.	43	43
5:00 p.m.	38	58
6:00 p.m.	40	68
7:00 p.m.	36	50
8:00 p.m.	56	45
9:00 p.m.	74	54
10:00 p.m.	85	74
11:00 p.m.	98	89

Figure 3: Hotel Parking Demand Profile

## Land Use: 710 General Office Building

### Time of Day Distribution for Parking Demand

Hour Beginning	Percent of Weekday Peak Parking Demand	
	General Urban/Suburban	Dense Multi-Use Urban
12:00–4:00 a.m.	–	–
5:00 a.m.	–	–
6:00 a.m.	–	–
7:00 a.m.	13	26
8:00 a.m.	48	65
9:00 a.m.	88	95
10:00 a.m.	100	100
11:00 a.m.	100	100
12:00 p.m.	85	99
1:00 p.m.	84	99
2:00 p.m.	93	97
3:00 p.m.	94	94
4:00 p.m.	85	90
5:00 p.m.	56	–
6:00 p.m.	20	–
7:00 p.m.	11	–
8:00 p.m.	–	–
9:00 p.m.	–	–
10:00 p.m.	–	–
11:00 p.m.	–	–

Figure 4: Office Parking Demand Profile

## Land Use: 850 Supermarket

Hour Beginning	Percent of Peak Parking Demand		
	Weekday	Saturday	Sunday
12:00–4:00 a.m.	–	–	–
5:00 a.m.	–	–	–
6:00 a.m.	–	–	–
7:00 a.m.	–	–	–
8:00 a.m.	–	–	8
9:00 a.m.	–	–	22
10:00 a.m.	59	70	50
11:00 a.m.	67	96	65
12:00 p.m.	86	99	85
1:00 p.m.	87	99	77
2:00 p.m.	93	97	85
3:00 p.m.	97	96	99
4:00 p.m.	97	100	100
5:00 p.m.	100	89	53
6:00 p.m.	99	–	42
7:00 p.m.	83	–	22
8:00 p.m.	53	–	13
9:00 p.m.	38	–	9
10:00 p.m.	20	–	3
11:00 p.m.	–	–	–

Figure 5: Supermarket Parking Demand Profile



## Land Use: 932 High-Turnover (Sit-Down) Restaurant

Hour Beginning	Percent of Weekday Peak Parking Demand		
	Family (breakfast, lunch, and dinner)	Family (lunch and dinner)	Lounge or Bar
12:00–4:00 a.m.	–	–	–
5:00 a.m.	–	–	–
6:00 a.m.	10	–	–
7:00 a.m.	25	–	–
8:00 a.m.	68	–	–
9:00 a.m.	72	–	–
10:00 a.m.	77	26	9
11:00 a.m.	83	43	15
12:00 p.m.	100	95	100
1:00 p.m.	91	95	81
2:00 p.m.	56	49	54
3:00 p.m.	42	39	33
4:00 p.m.	42	37	26
5:00 p.m.	64	62	29
6:00 p.m.	87	99	58
7:00 p.m.	79	100	70
8:00 p.m.	65	83	77
9:00 p.m.	42	51	61
10:00 p.m.	21	28	41
11:00 p.m.	–	–	–

Figure 6: Restaurant Parking Demand Profile – Weekday

## Land Use: 932 High-Turnover (Sit-Down) Restaurant

Hour Beginning	Percent of Saturday Peak Parking Demand		
	Family (breakfast, lunch, and dinner)	Family (lunch and dinner)	Lounge or Bar
12:00–4:00 a.m.	–	–	–
5:00 a.m.	–	–	–
6:00 a.m.	15	–	–
7:00 a.m.	28	–	–
8:00 a.m.	52	–	–
9:00 a.m.	75	–	–
10:00 a.m.	91	87	15
11:00 a.m.	100	90	23
12:00 p.m.	90	100	37
1:00 p.m.	80	98	50
2:00 p.m.	67	85	44
3:00 p.m.	45	73	37
4:00 p.m.	39	58	48
5:00 p.m.	40	63	64
6:00 p.m.	40	76	90
7:00 p.m.	58	78	100
8:00 p.m.	40	76	89
9:00 p.m.	35	55	71
10:00 p.m.	33	46	56
11:00 p.m.	–	–	–

Figure 7: Restaurant Parking Demand Profile – Weekend

The parking demand profiles have then been used to estimate the overall weekday and Saturday peak parking demand as calculated in **Table 3** and



**Table 4.**

It is noted that hotels rarely, if ever, operate at full capacity. Tourism Western Australia indicates that the hotel occupancy rate across WA ranged between 50% and 80% over the past 12 months. Carnarvon is within the Coral Coast tourism region which recorded an occupancy rate between 40% and 80%.

For the purposes of this assessment, the upper end of above range (80%) has been assumed to be the typical maximum occupancy rate for the proposed hotel. For a total of 96 units, this would equate to 77 occupied rooms or a parking requirement of 77 bays. The three pick-up / drop-off bays in the porte cochere have not been included as these will likely be used by the hotel only.

**Table 3: Peak Parking Demand Assessment - Weekday**

Hour Beginning	Land Use								Overall Bays Required
	Hotel (77 bays)		Office (23 bays)		Retail (32 bays)		Restaurant / Cafe (28 bays)		
	% Demand	Bays Required	% Demand	Bays Required	% Demand	Bays Required	% Demand	Bays Required	
5am	0%	0	0%	0	0%	0	0%	0	0
6am	89%	69	0%	0	0%	0	0%	0	69
7am	81%	62	13%	3	0%	0	0%	0	65
8am	72%	55	48%	11	0%	0	0%	0	66
9am	71%	55	88%	20	0%	0	0%	0	75
10am	48%	37	100%	23	59%	19	26%	7	86
11am	41%	32	100%	23	67%	21	43%	12	88
12pm	29%	22	85%	20	86%	28	95%	27	97
1pm	30%	23	84%	19	87%	28	95%	27	97
2pm	25%	19	93%	21	93%	30	49%	14	84
3pm	34%	26	94%	22	97%	31	39%	11	90
4pm	43%	33	85%	20	97%	31	37%	10	94
5pm	38%	29	56%	13	100%	32	62%	17	91
6pm	40%	31	20%	5	99%	32	99%	28	96
7pm	36%	28	11%	3	83%	27	100%	28	86
8pm	56%	43	0%	0	53%	17	83%	23	83
9pm	74%	57	0%	0	38%	12	51%	14	83
10pm	85%	65	0%	0	20%	6	28%	8	79
11pm	98%	75	0%	0	0%	0	0%	0	75

**Table 4: Peak Parking Demand Assessment - Saturday**

Hour Beginning	Land Use								Overall Bays Required
	Hotel (77 bays)		Office (23 bays)		Retail (32 bays)		Restaurant / Cafe (28 bays)		
	% Demand	Bays Required	% Demand	Bays Required	% Demand	Bays Required	% Demand	Bays Required	
5am	0%	0	0%	0	0%	0	0%	0	0
6am	76%	59	0%	0	0%	0	0%	0	59
7am	77%	59	0%	0	0%	0	0%	0	59
8am	74%	57	0%	0	0%	0	0%	0	57
9am	69%	53	0%	0	0%	0	0%	0	53
10am	50%	39	0%	0	70%	22	87%	24	85
11am	45%	35	0%	0	96%	31	90%	25	91
12pm	35%	27	0%	0	99%	32	100%	28	87
1pm	33%	25	0%	0	99%	32	98%	27	84
2pm	34%	26	0%	0	97%	31	85%	24	81
3pm	40%	31	0%	0	96%	31	73%	20	82
4pm	43%	33	0%	0	100%	32	58%	16	81
5pm	58%	45	0%	0	89%	28	63%	18	91
6pm	68%	52	0%	0	0%	0	76%	21	73
7pm	50%	39	0%	0	0%	0	78%	22	61
8pm	45%	35	0%	0	0%	0	76%	21	56
9pm	54%	42	0%	0	0%	0	55%	15	57
10pm	74%	57	0%	0	0%	0	46%	13	70
11pm	89%	69	0%	0	0%	0	0%	0	69

As shown, the non-residential peak parking demand is 97 bays on a weekday at around mid-day and 91 bays on a Saturday at 11am and 5pm.

The overall peak parking demand (residential and non-residential) is therefore 142 bays on a weekday and 135 bays on a Saturday.

The provision of 145 bays on the site would therefore accommodate the estimated peak parking demand of the development. The loss of three street parking bays is also compensated by the on-site bays as well as the pick-up / drop-off spaces that are replacing the street parking bays.

The following is also noted with regards to the parking demand:

- Some of the ground floor commercial space may eventually be used as offices which has a lower parking requirement and would therefore reduce the parking demand.
- No reduction has been applied for internal trips (hotel guests dining at the restaurant / café), multi-purpose trips (shop customer dining at the restaurant / café). Similarly, no reduction has been applied to account for the location of the development within the regional centre where many customers and visitors are within walking / cycling distance of the site and many will visit the site as well as neighbouring developments. If these factors were considered, then the parking demand would reduce further.
- For the Shop land use, LPS13 applies a greater parking requirement for development within the Regional Centre compared to developments in all other zones despite the availability of public parking. A development with a centre zone would typically require less on-site parking as some of the parking demand can be shared between neighbouring developments and accommodated within the public parking areas.
- There is a significant amount of public parking in the vicinity of the site.

### 3. Conclusion

A parking demand assessment for the proposed development at 85 Olivia Terrace in Carnarvon concluded the following:

- Based on the parking ratios outlined in the Shire's Local Planning Scheme, the development requires a minimum of 224 car parking bays including 42 resident bays and 182 commercial bays.
- Based on the mix of uses within the proposed development, it is likely that the peak hours of parking demand do not wholly overlap and there is potential for parking to be shared between the differing uses.
- A parking demand assessment estimates that the overall peak parking demand (residential and non-residential) is therefore 139 bays on a weekday and 133 bays on a Saturday.
- The provision of 145 bays on the site would therefore accommodate the estimated peak parking demand of the development.
- The following is also noted with regards to the parking demand:
  - Some of the ground floor commercial space may eventually be used as offices which has a lower parking requirement and would therefore reduce the parking demand.
  - No reduction has been applied for internal trips (hotel guests dining at the restaurant / café), multi-purpose trips (shop customer dining at the restaurant / café). Similarly, no reduction has been applied to account for the location of the development within the regional centre where many customers and visitors are within walking / cycling distance of the site and many will visit the site as well as neighbouring developments. If these factors were considered, then the parking demand would reduce further.
  - For the Shop land use, LPS13 applies a greater parking requirement for development within the Regional Centre compared to developments in all other zones despite the availability of public parking. A development with a centre zone would typically require less on-site parking as some of the parking demand can be shared between neighbouring developments and accommodated within the public parking areas.
  - There is a significant amount of public parking in the vicinity of the site.

## Schedule 2, Clause 77H(4) of the *Planning and Development (Local Planning Schemes) Regulations 2015* – Payment in lieu of parking condition for non-residential development

The first tranche of amendments to the *Planning and Development (Local Planning Schemes) Regulations 2015* (Regulations) that were published in the Gazette on 18 December 2020 included a new Part 9A for car parking provisions, which is to become operational on 1 July 2021 for the Perth Metropolitan and Peel Region Scheme Areas. The key parts to these provisions include parking exemptions for certain types of development and a standard and consistent approach to car parking variations, waivers, payment in lieu and shared parking.

Pursuant to Schedule 2, Clause 77G(2) of the Regulations, Local governments are required to approve a Payment in Lieu of Parking Plan (Plan) to apply payment in lieu conditions to development approvals for non-residential development. In preparing a Plan pursuant to Schedule 2, Clause 77J(2)(a), or adopting a Plan prepared by an owner of land pursuant to Schedule 2, Clause 77J(2)(b), the local government must use the method of calculation set out in the table below to calculate a reasonable estimate of costs that would be applied by way of a

condition to a development approval. In accordance with Schedule 2, Clause 77H(2), the maximum amount of payment required by the condition applied is the amount calculated utilising the method of calculation set out in the table below, with this not preventing a local government from applying a condition to a development approval that requires a lower payment, pursuant to Schedule 2, Clause 77H(3).

**Note:** The WAPC approved method of calculation does not take effect until 1 July 2021, which is when Part 9A of Schedule 2 of the Regulations comes into operation.

### Method of Calculation

#### Maximum Lump sum calculation (Infrastructure cost per m<sup>2</sup> x 15(m<sup>2</sup>)\*) x car parking space shortfall

\* The 15m<sup>2</sup> area represents a 50% discount to a car parking space and manoeuvring area of 30m<sup>2</sup>.

### Conditions of Use

- The infrastructure cost lump sum is to be established at the commencement of the Payment in Lieu of Parking Plan (Plan).
- The infrastructure cost lump sum requires indexing annually to maintain alignment with increases in infrastructure cost.
- Where lower cost infrastructure other than car parking spaces is targeted for delivery:
  - the 15(m<sup>2</sup>) component to the method of calculation is removed and substituted with a revised area representing a discount greater than 50% to a 30m<sup>2</sup> car parking space and manoeuvring area.
  - infrastructure required is to be based on an identified need for an area subject to a Plan.

The Department of Planning, Lands and Heritage acknowledges the traditional owners and custodians of this land. We pay our respect to Elders past and present, their descendants who are with us today, and those who will follow in their footsteps.

#### Disclaimer

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